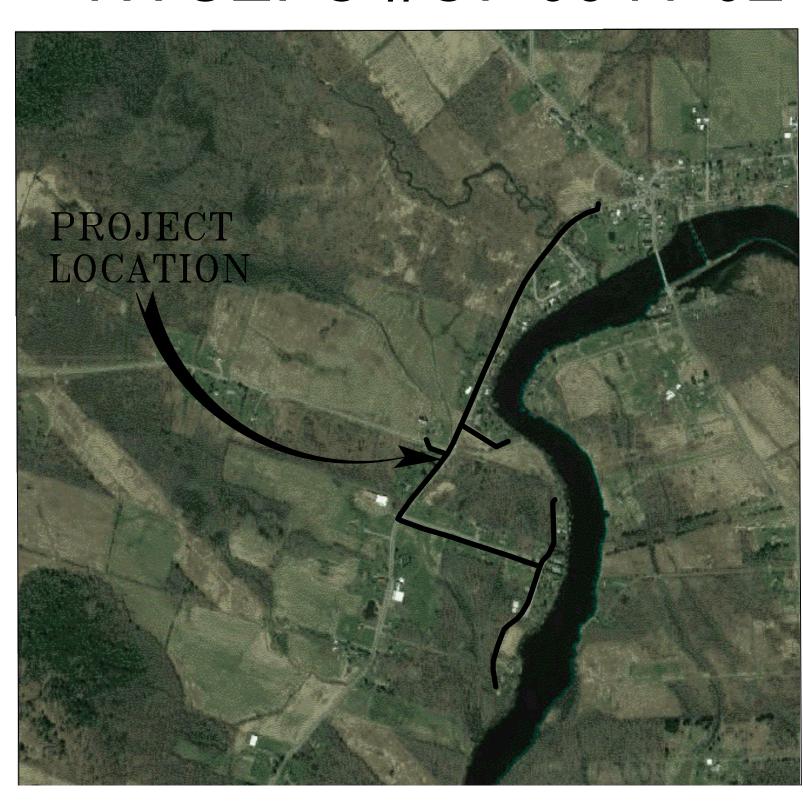
# CONTRACT DRAWINGS

FOR

# TOWN OF SCHROEPPEL AINSLEE DRIVE SEWER DISTRICT

OSWEGO COUNTY, NY AUGUST 2021 FINANCED BY USDA RURAL DEVELOPMENT AND NYSEFC #C7-6341-02-00



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Sheet List Table					
Sheet Number	Sheet Title				
G-001	COVER SHEET				
G-002	GENERAL NOTES				
G-003	GENERAL NOTES				
C-100	COUNTY ROUTE 12 - PLAN & PROFILE				
C-101	COUNTY ROUTE 12 - PLAN & PROFILE				
C-102	COUNTY ROUTE 12 - PLAN & PROFILE				
C-103	STEWARTS CORNERS RD & SUNRISE DR - PLAN & PROFILE				
C-104	AINSLEE DRIVE - PLAN & PROFILE				
C-105	AINSLEE DRIVE - PLAN & PROFILE				
C-106	AINSLEE DRIVE - PLAN AND PROFILE				
C-107	HICKORY LANE - PLAN & PROFILE				
C-500	DETAILS				
C-501	DETAILS				
C-502	DETAILS				
C-503	DETAILS				
C-504	DETAILS				

PROJECT MAP

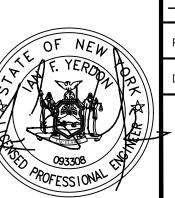
SCALE: NTS

ATLANTIC OCEAN

NEW YORK LOCATION MAP

AMERICAN IRON AND STEEL (AIS) PROJECT

RECORD DRAWINGS THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST



G-001

WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT



#### GENERAL NOTES

- 1. TOPOGRAPHIC INFORMATION BASED ON SURVEYS BY LAFAVE, WHITE, AND MCGIVERN. THERESA, NY.
- 2. THE CONTRACTOR SHALL LOCATE, MARK, SAFEGUARD AND PRESERVE ALL SURVEY CONTROL MONUMENTS AND R.O.W. MONUMENTS IN THE AREAS OF CONSTRUCTION. 3. EXISTING UNDERGROUND UTILITIES SHOWN ARE APPROXIMATE IN LOCATION AND ELEVATION. THE LOCATION SHOWN IS BASED ON ABOVE GROUND FEATURES AND CONSTRUCTION

BLUEPRINTS. OTHERS MAY EXIST. ALL LOCAL UTILITY COMPANIES SHOULD BE NOTIFIED

4. THE CONTRACTOR SHALL DETERMINE EXACT LOCATION AND ELEVATION OF UNDERGROUND UTILITIES BEFORE COMMENCING CONSTRUCTION. CONTRACTOR SHALL MAKE EXPLORATION EXCAVATIONS TO LOCATE EXISTING UNDERGROUND FACILITIES SUFFICIENTLY AHEAD OF

CONSTRUCTION TO PERMIT REVISIONS AS REQUIRED TO MEET EXISTING CONDITIONS.

- 5. THE CONTRACTOR SHALL FIELD VERIFY THE EXACT LOCATION OF THE EXISTING SEWER SYSTEM IN AREAS WHERE THE NEW INSTALLATION IS SHOWN CONNECTING TO THE EXISTING SYSTEM.
- THE LOCATION OF THE PROPOSED SEWER MAIN AND LATERALS SHOWN IS APPROXIMATE. THE ACTUAL LOCATION OF THE SEWER MAIN AND LATERALS WILL BE GOVERNED BY THE ACTUAL LOCATION OF EXISTING UNDERGROUND UTILITIES, STRUCTURES AND OTHER CONTROLLING FACTORS, AS DETERMINED BY THE ENGINEER DURING CONSTRUCTION. CONTRACTOR SHALL COORDINATE LOCATION AND DEPTH OF LATERALS AND GRINDER PUMPS WITH INDIVIDUAL PROPERTY OWNERS AND OBTAIN PROPERTY OWNERS APPROVAL PRIOR TO CONSTRUCTION. GRINDER PUMPS SHALL BE INSTALLED AT A PROPER ELEVATION SUCH THAT GRAVITY SEWER LATERALS CAN BE INSTALLED FROM HOMES/STRUCTURES AT A MINIMUM 2% SLOPE AND CONNECTED TO PVC INLET STUB. ALL WORK NOT INSTALLED IN ACCORDANCE WITH THE REQUIREMENTS SHALL BE CORRECTED AT NO ADDITIONAL
- MINIMUM SEPARATION BETWEEN WATER, SEWER, AND STORM MAINS TO BE 24" VERTICALLY MEASURED FROM THE OUTSIDE OF THE PIPES AT THE POINT OF THE CROSSING. MINIMUM HORIZONTAL SEPARATION BETWEEN WATERMAINS AND SEWER MAINS, MANHOLES OR VAULTS TO BE 10 FEET MEASURED FROM THE OUTSIDE OF THE PIPES, MANHOLES OR VAULTS. ONE FULL LENGTH OF SEWER MAIN SHALL BE CENTERED UNDER OR OVER THE WATER SO THAT BOTH JOINTS WILL BE AS FAR FROM THE WATER AS POSSIBLE. WHERE A SEWER MAIN CROSSES UNDER ANOTHER UTILITY SEWER, ADEQUATE STRUCTURAL SUPPORT (COMPACTED SELECT FILL) SHALL BE PROVIDED FOR THE UTILITY TO PREVENT EXCESSIVE DEFLECTION OF JOINTS AND
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE, REPAIRS AND/OR REPLACEMENT OF SIDEWALKS, DRIVEWAYS, CURBING, GUTTERS, ROADWAYS, DRAINAGE STRUCTURES, AND OTHER STRUCTURES WHICH MAY BE REQUIRED TO CONSTRUCT THE PROJECT.
- CONTRACTOR TO USE 2-45° BENDS IN PLACE OF 90° BENDS ON ALL PRESSURE PIPING SYSTEMS WHENEVER POSSIBLE.
- 10. RESTRAINED JOINTS SHALL BE USED FOR ONE FULL PIPE LENGTH ON EACH SIDE OF A 45° BEND AND FOR FOUR FULL PIPE LENGTHS ON EACH SIDE OF A 90° BEND. RESTRAINED JOINTS SHALL ALSO BE USED FOR FIVE FULL LENGTHS WHEN CONNECTING TO HDPE PIPE.
- 11. THE CONTRACTOR SHALL PAY FOR ALL WATER USED FOR TESTING, CLEANUP AND DUST CONTROL. WITHDRAWAL OF WATER SHALL ONLY BE DONE FROM PREDESIGNATED LOCATIONS. CONTRACTOR SHALL USE A BACKFLOW PREVENTION DEVICE AND METER WHEN WITHDRAWING WATER FROM THE
- 12. THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE AUTHORITIES, (NYSDOT, VILLAGE, COUNTY, TOWN) 48 HOURS PRIOR TO ANY ROAD CROSSINGS (JACKING/BORINGS, DIRECTIONAL DRILLING, SERVICE PUSHES OR OPEN CUTS). NO OPEN CUTTING PAVEMENT WITHIN 10 FEET FROM THE EDGE OF SHOULDER INSIDE THE NYSDOT RIGHT-OF-WAY (EXCEPTIONS MUST BE APPROVED BY NYSDOT PRIOR TO EXCAVATION).
- 13. MINIMUM COVER FOR PROPOSED SEWER MAIN, LATERALS, AND ALL APPURTENANCES SHALL BE 5 FEET UNLESS OTHERWISE NOTED.
- 14. ALL GRINDER PUMPS SHALL BE 240V, SINGLE PHASE, UNLESS OTHERWISE SPECIFIED.
- 15. GRINDER PUMPS WITH WATERTIGHT, FLOOD PLAIN LIDS AND VENTS SHALL BE INSTALLED WHEN THE FINISHED GRADE OF THE FINAL, GRINDER LOCATION IS BELOW 371 FEET IN ELEVATION (FIELD VERIFIED BY CONTRACTOR)
- 16. ALL LATERAL SIZES ARE 1.25 INCH DIAMETER, UNLESS OTHERWISE SPECIFIED.
- 17. TRACK VEHICLES ARE PROHIBITED FROM TRAVEL LANES AND SHOULDERS.
- 18. IF APPLICABLE, OBTAIN NECESSARY VILLAGE, TOWN & COUNTY ROAD CONSTRUCTION PERMIT.
- 19. ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES.
- 19. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES.
- 20. NOTIFY DIG SAFELY NEW YORK THREE (3) WORK DAYS PRIOR TO DIGGING, DRILLING, OR BLASTING AT 1-800-962-7962 FOR A UTILITY STAKE-OUT.
- 21. DEBRIS SHALL BE KEPT OUT OF PIPE AT ALL TIMES DURING CONSTRUCTION.
- 22. THE CONTRACTOR SHALL APPLY A 2" TOP COATING OF GRAVEL AND/OR CRUSHED STONE TO ALL UNPAVED ROADS, FOR THE ENTIRE WIDTH OF THE ROAD, THAT PARALLEL AND ARE ADJACENT TO SEWER MAIN INSTALLATION. MATERIAL TYPE SHALL MATCH EXISTING OR AS DIRECTED BY ENGINEER. ANY REQUIRED TOP COATING TO BE INCLUDED IN THE RESTORATION PAYMENT ITEM.
- 23. NO WORK SHALL TAKE PLACE ON TOWN, COUNTY, OR STATE ROADS WHEN SNOW PLOWS ARE OUT
- 24. SHOULD FINAL PAVING OF ROADWAY AND DRIVEWAY CUTS NOT BE COMPLETED PRIOR TO WINTER, THEN TEMPORARY PAVING MUST BE DONE.
- 25. ALL SILT FENCE AND EROSION CONTROL MEASURES SHALL BE INSTALL PRIOR TO SEWER MAIN INSTALLATION.
- 26. ALL TREE REMOVAL SHALL BE LIMITED TO THE TIME PERIOD BETWEEN NOVEMBER 1ST AND APRIL

#### | DEDICATED FLAGGER REQUIRED TOWN AND COUNTY ROADS

CALL DIG SAFELY NEW YORK BEFORE DIGGING @ 1-800-962-7962

#### PROPOSED FEATURES LEGEND

PROPOSED SANITARY SEWER FORCEMAIN PROPOSED SANITARY SEWER FORCEMAIN W/ TRENCHLESS ROAD CROSSING TEST PIT

> PROPOSED TEMPORARY EASEMENT PROPOSED PERMANENT EASEMENT

---<del>===</del>---PROPOSED SANITARY SEWER FORCEMAIN LATERAL W/ TRENCHLESS ROAD CROSSING

PROPOSED SANITARY SEWER FORCEMAIN LATERAL -----

GRINDER PUMP

GRINDER PUMP WITH FLOODPLAIN PROTECTION (FIELD VERIFIED LOCATIONS UNDER 371 FEET ELEVATION)

SANITARY SEWER CLEANOUT

COMBINATION AIR RELEASE VALVE STATION SERVICE LATERAL LOCATION FORM ON FILE

#### 

<u>EXIS</u>	<u>ting features</u>	LEGE	<u>END</u>
	PAVED ROAD/PAVED DRIVE	·	MAIL BOX
	UNPAVED ROAD/UNPAVED DRIVE	0	FILLER CAP
-	GUIDE RAIL		POWER POLE
	SIDEWALK	$\sim$	LIGHT POLE
TRAIL	TRAIL	$\odot$	NON UTILITY POLE
	BRIDGE STRUCTURE	UB	UTILITY BOX
	BUILDING	ф <b>—</b> Ж	UTILITY POLE W LIGHT
DECK,	DECK WITH STEPS	<del>-0-</del>	SINGLE POST SIGN
(P00)_	POOL	00	TWO POST SIGN
	SWAMP	$\circ$	POST
RW	RETAINING WALL	CB	CATCH BASIN SQUARE
	FREESTANDING WALL	(CB)	CATCH BASIN ROUND
120	MAJOR CONTOUR	0	MANHOLE
\	MINOR CONTOUR	>	CULVERT
	DEPRESSION CONTOUR	$\approx$	RAPIDS
PILE	PILE/DEBRIS OUTLINE	$\langle \cdot \rangle$	DECIDUOUS TREE
<del></del>	FENCE	*	CONIFEROUS TREE
	STONE WALL	$\odot$	DECIDUOUS BUSH
	AGRICULTURAL FIELD	*	CONIFEROUS BUSH
	WOODS LINE		BOULDER
	BRUSH LINE	^^	ROCK OUTCROP

RKOZH TIME HEDGE LINE \_..\_ DITCH WATER EDGE PROPERTY LINES \_\_\_\_\_ ------ ST ------

UNDERGROUND ELECTRIC/TELEPHONE LINE ----- UE/T -----UNDERGROUND ELECTRIC LINE ----- UGE -----

ELECTRICAL METER

PEM5E

\_\_\_\_w\_\_ SATELLITE DISH PROPANE TANK GAS METER

WATER VALVE WATER SHUTOFF/CURBBOX

APPROXIMATE LOCATION OF WELL SANITARY SEWER MANHOLE 100' WETLAND BUFFER FEDERAL WETLAND BUFFER

UNIDENTIFIED OBJECT

FIRE HYDRANT

NGAS MARKER

BOLLARD

FIRE PIT

STUMP

GUY WIRE

FLAG POLE

ELECTRICAL PANEL FEDERAL WETLAND WITH NAME

A/G	ABOVE GROUND	HW	HEAD WALL
ASPH	ASPHALT	IP	IRON PIN
CB	CATCH BASIN	LP	LIGHT POLE
CONC	CONCRETE	LS	LANDSCAPING
CS	CRUSHED STONE	MB	MAILBOX
DK	DECK	OBS	OBSTRUCTED
FB	FLOWER BOX	OBSTR	OBSTRUCTED
FI	FIELD INLET	RW	RETAINING WALL
FM	FLUSH MEDIAN	SAT	SATELLITE DISH
FP	FLAG POLE	UB	UTILITY BOX
GRA	GRAVEL	ROW	RIGHT OF WAY
GV	GAS VALVE	WSO	WATER SHUT OFF
EX	EXISTING	WV	WATER VALVE
SW	SIDEWALK	FB	FOOT BRIDGE
TS&V	TAPPING SLEEVE AND VALVE COMPLETE	CU	COPPER
NYSDOT	NEW YORK STATE DEPARTMENT OF TRANSPORTATION	GM	GAS METER

RECORD DRAWINGS THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR. THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED TO BE CORRECT IN EACH AND EVERY DETAIL.

70 MAIN STREET, PO BOX 368, CANTON, NY 13617



TION DIST ÞΩ ШЩ S XX XX XX

REVISIONS

REV DESCRIPTION DA FOR RECORD

PROJ. #: AUGUST

> SHEET G-002

THE LOCATION, SIZES AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN ON THIS DRAWING WERE OBTAINED FROM AVAILABLE DRAWINGS AND EVIDENCE OF ABOVE GROUND FEATURES. THEIR LOCATION, SIZE AND ELEVATION MAY NOT BE ENTIRELY TRUE AND CORRECT. OTHER UTILITIES MAY EXIST. ALL LOCAL UTILITY COMPANIES SHOULD BE NOTIFIED BEFORE EXCAVATION.

THE PROPERTY LINES SHOWN ON THIS DRAWING WERE OBTAINED FROM

TO SUCH VARIATIONS AND CORRECTIONS AS MIGHT RESULT FROM AN

TRUE AND CORRECT AND ARE NOT INTENDED FOR USE IN THE CONVEYANCE

OF LAND. THE ACTUAL LOCATION OF THESE PROPERTY LINES IS SUBJECT

COUNTY TAX MAPS. THEIR LOCATION MAY NOT BE ENTIRELY

ACCURATE INSTRUMENT SURVEY.

NOTE:

2-LANE 2-WAY ROADWAY

NOT TO SCALE

(SEE NOTE

**◆↓** 

WORK

TRAVEL LANE

TRAVEL LANE

SEE LEGEND & TABLES STANDARD SHEET (M619-11) FOR LEGEND OF SYMBOLS AND/OR LETTER CODES USED IN THIS DRAWING

LONGITUDINAL BUFFER SPACE					
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE				
25	50 m				
30	60 m				
35	75 m				
40	95 m				
45	110 m				
50	130 m				
55	150 m				
60	175 m				
65	200 m				

TRAVEL LANE <

TRAVEL LANE ⇒

1000' POSTED SPEED LIMIT

1. AT SIGNALIZED INTERSECTIONS, SIGNALS SHALL BE TURNED OFF FOR ANY FLAGGING OPERATION, UNLESS

WHEN PAEVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED, CHANNELIZED DEVICES SHALL

BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY.

CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK

. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE

WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE

ONE-LANE, TWO-WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES.

THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROX. 1/2 THE DISTANCE BETWEEN THE

TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING. B. FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL 2 ILLUMINATION DURING NIGHT TIME

ALL FLAGGERS SHALL USE 24" (MIN) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6' STAFF.

11. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE CONSTRAINTS EXIST.

12. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLE, OR MATERIAL SHALL OCCUR WITHIN THE

BUFFER SPACE AT ANY TIME.

13. FOR PEDESTRIAN DETOUR ACCOMMODATIONS REFER TO THE NYSDOT STANDARD SHEETS TITLED

10. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED

T ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK

FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR

TABLE 6C-3 TAPER LENGTH CRITERIA FOR TEMPORARY TRAFFIC CONTROL ZONES			
TYPE OF TAPER  MERGING TAPER	TAPER LENGTH (L)		
SHIFTING TAPER	L/2		
SHOULDER TAPER ONE—LANE, TWO—WAY TRAFFIC TAPER	30 m MAXIMUM		
DOWNSTREAM TAPER	30 m PER LANE		

ISTANCE					
TOTANCE	BETWEEN	N SIGNS	SIGN LEGEND		
A (m)	B (m)	C (m)	XX	YY	
30	30	30	AHEAD	AHEAD	
60	60	60	AHEAD	AHEAD	
100	100	100	1000 FT	AHEAD	
150	150	150	1500 FT	1000 FT	
300	450	800	1 MILE	1/2 MILE	
	(m) 30 60 100 150	(m)     (m)       30     30       60     60       100     100       150     150	(m)     (m)     (m)       30     30     30       60     60     60       100     100     100       150     150     150	(m)         (m)         (m)         XX           30         30         30         AHFAD           60         60         60         AHEAD           100         100         100         FT           150         150         150         150         FT	

WORK

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS						
SPEED LIMIT (S) (km/h)	TAPER LENGTH (L) (METERS)	L = TAPER LENGTH				
60 km/h (40 MPH) OR LESS	$L = WS^2/155$	W = WIDTH OF OFFSET (m) S = PRE CONSTRUCTION POSTED				
70 km/h (45 MPH) OR MORE	L = WS/1.6	SPEED LIMIT (km/h)				
STANDARD TAPER LENGTHS						

LATERAL SHIFT	TEMPORARY TRAFFIC CONTROL ZONE PRE CONSTRUCTION POSTED SPEED LIMIT								
OF TRAFFIC FLOW PATH	40 km/h (25 MPH)	50 km/h (30 MPH)	60 km/h (35 MPH)	60 km/h (40 MPH)	70 km/h (45 MPH)	80 km/h (50 MPH)	90 km/h (55 MPH)	100km/h (60 MPH)	110km/h (65_MPH)
1.22 m	15 m	20 m	30 m	30 m	55 m	65 m	70 m	80 m	85 m
1.52 m	20 m	25 m	40 m	40 m	70 m	80 m	90 m	95 m	105 m
1.83 m	20 m	30 m	45 m	45 m	85 m	95 m	105 m	115 m	130 m
2.13 m	25 m	35 m	50 m	50 m	95 m	110 m	120 m	135 m	150 m
2.44 m	30 m	40 m	60 m	60 m	110 m	125 m	140 m	155 m	170 m
2.74 m	30 m	45 m	65 m	65 m	120 m	140 m	155 m	175 m	190 m
3.05 m	35 m	50 m	75 m	75 m	135 m	155 m	175 m	195 m	210 m
3.36 m	35 m	55 m	80 m	80 m	150 m	170 m	190 m	210 m	235 m
3.66 m	40 m	60 m	90 m	90 m	165 m	185 m	210 m	230 m	255 m

## SPECIAL NOTES

- 1. ALL CULVERTS DISTURBED OR DAMAGED AS A RESULT OF THE WORK WILL BE REPLACED WITH AN APPROVED CULVERT EQUAL TO OR BETTER, AS DIRECTED BY NYSDOT INSPECTOR
- 2. ALL DITCH LINES AND DRAINAGE STRUCTURES SHALL BE MAINTAINED CLEAN AND OPERATIVE DURING CONSTRUCTION.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR RESETTING OR REPLACING ROW MONUMENTS DISTURBED OR DAMAGED AS A RESULT OF THE WORK.
- BLASTING ON OR IMMEDIATELY ADJACENT TO DOT ROW WILL NOT BE ALLOWED WITHOUT WRITTEN APPROVAL FROM D.O.T. REGIONAL OFFICE.
- PERMITTEE SHALL BE RESPONSIBLE FOR WORK ZONE TRAFFIC CONTROL AT ALL TIMES FOR THE DURATION OF THE PERMITTED WORK. A SHOULDER CLOSURE SHALL BE IN PLACE ANY TIME WORKER OR CONSTRUCTION EQUIPMENT IS WITHIN THE RIGHT-OF-WAY IMMEDIATELY OUTSIDE THE SHOULDER AREA.
- PEDESTRIAN ACCOMMODATIONS SHALL BE MAINTAINED FOR THE DURATION OF THE PROPOSED WORK. ANY DISTURBED AREAS SHALL BE ADEQUATELY FENCED TO PREVENT PEDESTRIAN ACCESS WHEN THE CONTRACTORS OPERATIONS ARE SHUT DOWN.
- ALL DISTURBED AREAS WITHIN THE RIGHT-OF-WAY ARE TO BE TOPSOILED, SEEDED AND MULCHED PRIOR TO CLOSE OF BUSINESS EVERY FRIDAY. IN CASE OF INCLEMENT WEATHER, THE AREA SHALL BE RESTORED BEFORE ANY FURTHER EXCAVATION TAKES PLACE ON THE NEXT BUSINESS DAY.
- MATERIALS, EQUIPMENT AND VEHICLES SHALL NOT BE STORED OR PARKED WITHIN THE STATE RIGHT-OF-WAY BEFORE WORK BEGINS OR AFTER CONTRACTOR'S OPERATIONS ARE SHUT DOWN. STAGING AREAS OUTSIDE THE RIGHT-OF-WAY SHALL BE USED TO STOCKPILE ALL CONSTRUCTION
- 9. HIGH VISIBILITY SAFETY APPAREL AND HARD HATS SHALL BE WORN BY ALL PERSONS AT ALL TIMES WHEN WORKING IN THE RIGHT-OF-WAY. SUPERVISORY PERSONNEL, OWNERS, DELIVERY PEOPLE, AND ANY VISITORS TO THE PROJECT ARE NOT EXEMPT FROM THIS REQUIREMENT.
- 10. CARE SHALL BE TAKEN TO INSURE THAT NO DAMAGE OCCURS TO THE EXISTING PAVEMENT, SHOULDER, AND/OR CURB AREAS AS A RESULT OF CONSTRUCTION EQUIPMENT MOVEMENT.
- 11. SHOULD THE EXISTING RIGHT-OF-WAY MARKERS BE DAMAGED OR DISTURBED IN ANY WAY AS A RESULT OF CONSTRUCTION OPERATIONS, THE MARKERS SHALL BE REPLACED OR RESET, AT NO COST TO THE OWNER OR AUTHORITY HAVING JURISDICTION, IN ACCORDANCE WITH SECTION 625 OF THE NYSDOT STANDARD SPECIFICATIONS AND STANDARD SHEET 625.01.
- PERMITTEE SHALL ENSURE THAT ROADSIDE DRAINAGE WILL BE MAINTAINED AND PROTECTED AT ALL TIMES DURING CONTRACTOR'S WORK OPERATIONS. ALL WORK AFFECTING RUNOFF FROM THE WORK SITE SHALL INVOLVE PROPER USE OF SEDIMENTATION AND EROSION CONTROL MEASURES.
- 13. NO SECTIONS OF GUIDERAIL WITHIN THE RIGHT-OF-WAY MAY BE REMOVED AT ANY TIME DURING THE COURSE OF THE WORK.

NOTES FOR SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE FOR 2-LANE 2-WAY ROADWAY WHEN PAVED SHOULDERS HAVING A WIDTH OF 8' OR MORE ARE CLOSED. CHANNELIZED DEVICES SHAL BE USED TO CLOSE THE SHOULDER IN ADVANCE TO DELINEATE THE BEGINNING OF THE WORK AREA AND DIRECT VEHICULAR TRAFFIC TO REMAIN IN THE TRAVEL WAY. WHEN A SIDE ROAD OR DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK 3. NO WORK ACTIVITY, EQUIPMENT, OR STORAGE OF VEHICLE, OR MATERIAL SHALL OCCUR WITHIN THE BUFFER SPACE AT ANY TIME

4. CHANNELIZING DEVICE SPACING (CENTER TO CENTER) SHALL NOT EXCEED 40' IN THE ACTIVE WORK 5. TRANSVERSE DEVICES SHALL BE REQUIRED (AS PER NYSDOT 619 STANDARD SPECIFICATIONS) WHEN A PAVED SHOULDER HAVING A WIDTH OF 8' OR GREATER IS CLOSED FOR A DISTANCE GREATER THAN 6. THE END ROAD WORK SIGN (G20-2) SHALL BE PLACED A MAXIMUM OF 500' PAST THE END OF THE WHERE DIRECTED BY THE ENGINEER, A BUFFER SPACE SHALL BE PROVIDED IN ORDER TO LOCATE THE ONE—LANE, TWO—WAY TRAFFIC TAPER PRIOR TO ANY HORIZONTAL OR VERTICAL CURVE, IN ORDER TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND/OR A QUEUE OF STOPPED VEHICLES. . THE FLAG TREE SHALL BE LOCATED ON THE SHOULDER, AT APPROX. 1 THE DISTANCE BETWEEN THE FLAGGER SIGN (W20-7a) AND THE FLAGGER. 9. FLAGGER SIGN (W20-7a) AND ONE LANE ROAD AHEAD SIGN (W20-4) SHALL BE REMOVED, COVERED OR

TURNED AWAY FROM ROAD USERS WHEN FLAGGING OPERATIONS ARE NOT OCCURRING.

10. FLAGGER AND FLAG TREE SHALL BE ILLUMINATED TO LEVEL 2 ILLUMINATION DURING NIGHT TIME ALL FLAGGERS SHALL USE 24" (MIN) OCTAGON SHAPED STOP/SLOW PADDLES HAVING 6' STAFF. 12. CENTERLINE CHANNELIZING DEVICES ARE OPTIONAL AND MAY BE ELIMINATED WHERE SPACE

WORK	ZONE TRAFFIC CONTROL LEGEND					
SYMBOL	DESCRIPTION					
DIRECTION OF TRAFFIC						
• •	ARROW PANEL, CAUTION MODE					
	CHANNELIZING DEVICE					
<b>+</b>	SIGN, TEMPORARY					
	WORK SPACE					
WORK VEHICLE W/ TRUCK MOUNTED ATTENUATOR						

#### \* PRECONSTRUCTION POSTED SPEED LIMIT

URBAN: (MEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 DRIVEWAYS/ML MINOR COMMERCIAL DRIVEWAY DENSITIES 10 DRIVEWAYS/MI OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEŔOUS RIGHT OF WAY CONSTRAINTS, HIGH DENSITY OF CROSS STREETS, 85TH PERCENTILE SPEEDS OF

ANY AREA NOT EXHIBITING MORE THAN ONE OF ABOVE CHARACTERISTICS.

DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.

LOCAL OR INTERREGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

#### GENERAL NOTES

- 1. NO SIGNS SHALL BE REMOVED FROM THE RIGHT-OF-WAY.
- ROAD TO BE KEPT CLEAN OF MUD AND DEBRIS AT ALL TIMES
- 3. ROADSIDE DRAINAGE TO BE MAINTAINED AT ALL TIMES
- 4. MATERIALS, EQUIPMENT AND VEHICLES ARE NOT TO BE STORED OR PARKED WITHIN THE RIGHT-OF-WAY.
- 5. NOTIFY DIG SAFELY NEW YORK THREE WORKING DAYS PRIOR TO DIGGING DRILLING, OR BLASTING AT 1-800-962-7962, FOR A UTILITY STAKE-OUT.
- 6. ALL WORK CONTEMPLATED AND MATERIALS USED WITHIN THE RIGHT-OF-WAY SHALL BE COVERED BY AND IN CONFORMITY WITH THE LATEST EDITION OF THE NYS DEPARTMENT OF TRANSPORTATION SPECIFICATIONS BOOK AND ANY SUBSEQUENT ADDENDA ALONG WITH ANY APPROPRIATE CURRENT NYS DEPARTMENT OF TRANSPORTATION STANDARD SHEETS, EXCEPT AS MODIFIED IN THESE PLANS AND IN THE ITEMIZED PROPOSAL. METRIC UNITS MAY BE CONVERTED TO ENGLISH.
- 7. QUALITY CONTROL OF ASPHALT CONCRETE SHALL MEET THE REQUIREMENTS OF SECTION 402 OF THE NYSDOT STANDARD SPECIFICATIONS. ASPHALT COURSE DEPTHS SHOWN ON THE PLANS ARE COMPACTED DEPTHS.
- 8. NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE AUTHORITY HAVING JURISDICTION. ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING.
- 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ANY HIGHWAY WORK PERMIT, ASSOCIATED BOND, AND SPECIAL INSPECTION FOR WORK WITHIN THE RIGHT-OF-WAYS. THE TOWN SHALL BE CO-PERMITEE FOR THE HIGHWAY WORK PERMITS.
- 10. ESTABLISHING TURF SHALL BE ACCOMPLISHED PRIOR TO OCTOBER 15TH. SHOULD THERE BE ANY OPEN AREAS AFTER THAT DATE, THOSE AREAS SHALL BE APPROPRIATELY PROTECTED FOR EROSION THROUGH THE WINTER PER SWPPP REQUIREMENTS.
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPAIR ANY PROJECT RELATED SETTLING OF LAWN AREAS, SIDEWALKS, DRIVEWAYS, ROADWAY, AND ANY OTHER AREAS OF DISTURBANCE UNTIL FINAL CONTRACT ACCEPTANCE.

#### RECORD DRAWINGS THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED TO BE CORRECT IN EACH AND EVERY DETAIL.

70 MAIN STREET, PO BOX 368, CANTON, NY 13617

#### HOLIDAY RESTRICTIONS

SHOULDER CLOSURES AND LANE RESTRICTIONS ARE NOT ALLOWED DURING THE FOLLOWING DATES:

• MEMORIAL DAY HOLIDAY - FRIDAY, MAY 23, 2020 THROUGH TUESDAY, MAY 26, 2020 • INDEPENDENCE DAY HOLIDAY - THURSDAY, JULY 2, 2020 THROUGH MONDAY, JULY 6, 2020 • OSWEGO HARBORFEST - THURSDAY, JULY 23, 2020 THROUGH SUNDAY, JULY 26, 2020 • LABOR DAY HOLIDAY - FRIDAY, SEPTEMBER 4, 2020 THROUGH MONDAY, SEPTEMBER 7, 2020 • THANKSGIVING HOLIDAY - WEDNESDAY, NOVEMBER 25, 2020 THROUGH MONDAY, NOVEMBER 30, 2020 CHRISTMAS HOLIDAY - THURSDAY, DECEMBER 24, 2020 THROUGH SUNDAY, DECEMBER 27, 2020 • NEW YEAR'S HOLIDAY - THURSDAY, DECEMBER 31, 2020 THROUGH SUNDAY, JANUARY 3, 2020

#### TYPICAL WORK ZONE TRAFFIC CONTROL NOTES

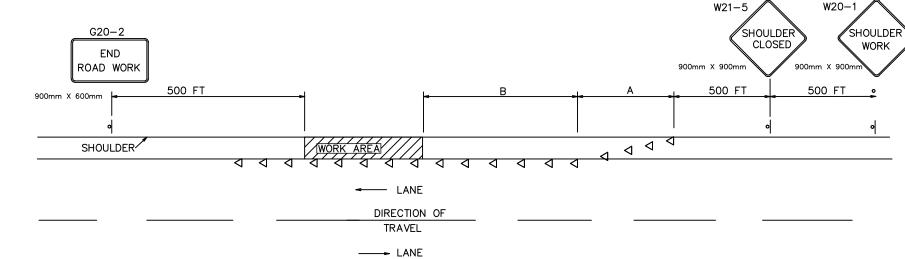
- 1. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS -LATEST EDITION AND THE NEW YORK STATE SUPPLEMENT.
- 2. PRIOR TO THE START OF WORK, THE CONTRACTOR MUST SUBMIT ANY PROPOSED CHANGES TO THE TRAFFIC CONTROL PLAN TO THE ENGINEER FOR APPROVAL.
- THE TRAFFIC MAINTENANCE SCHEMES SHOWN IN THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT DESCRIBE THE MINIMUM METHODS AND CONTROL DEVICES NECESSARY. THE ENGINEER MAY ORDER ADDITIONAL DEVICES AND/OR METHODS TO MEET FIELD CONDITIONS.

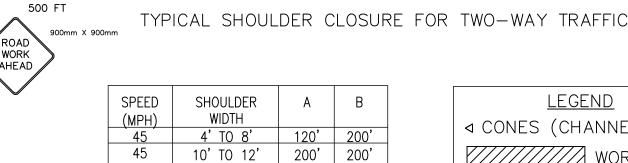
NO ADDITIONAL PAYMENT WILL BE MADE FOR ADDITIONAL DEVICES SO ORDERED.

- 4. DIAMOND-SHAPED SIGNS SHALL BE USED FOR ALL ADVANCE WARNING SIGNS SHOWN IN PART 6 OF THE NATIONAL MUTCD ALONG WITH THE NEW YORK STATE SUPPLEMENT. COLOR REQUIREMENTS SHALL BE BLACK TEXT ON ORANGE BACKGROUND. COLOR REQUIREMENTS FOR GUIDE SIGNS AND REGULATORY SIGNS USED FOR CONSTRUCTION SIGNING SHALL BE AS SPECIFIED IN THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT FOR EACH SIGN,
- THE CORRECT SEQUENCE AND SPACING OF SIGNS, EITHER PERMANENT OR TEMPORARY MUST BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT, UNLESS SHOWN OTHERWISE ON THE PLANS.
- ALL SIGNS, INCLUDING GUIDE SIGNS, SHALL INDICATE ACTUAL CONDITIONS AT ALL ITEMS AND SHALL BE COVERED, MOVED, REMOVED OR CHANGED IMMEDIATELY AS DIRECTED BY THE

ENGINEER.

- WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH THE MOST RECENT NYSDOT STANDARD SPECIFICATIONS-SECTION 619 WORK ZONE TRAFFIC CONTROL, THE NATIONAL MUTCD AND THE NEW YORK STATE SUPPLEMENT, AND ANY PROVISIONS CONTAINED IN THE PLANS AND/OR PROPOSAL OF THIS PERMIT OR AS ORDERED BY THE AUTHORITY HAVING
- 8. WORK ZONES IN OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP. WORK ZONE IS DEFINED AS THAT AREA IN WHICH TRAFFIC IS RESTRICTED BECAUSE OF CONSTRUCTION ACTIVITIES, OR THAT AREA WHICH INVOLVES A DROP-OFF NEXT TO THE PAVEMENT.
- 9. CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE CLEAR ROADSIDE AREA DURING NON-WORKING HOURS. PROVIDE A MINIMUM 30 FOOT CLEAR OFFSET.
- 10. NO MATERIAL IS TO BE PLACED ON THE SHOULDER OR WITHIN THE CLEAR ROADSIDE AREA (DETERMINED AS IN NOTE 9) EXCEPT THAT WHICH IS TO BE PLACED THAT DAY.
- 11. PRIVATE VEHICLES OWNED BY THE CONTRACTOR OR HIS WORKMEN SHALL NOT BE PARKED ON THE PAVEMENT OR THE SHOULDERS, OR ANY OTHER LOCATION CONSIDERED BY THE ENGINEER TO BE A HAZARD.
- 12. THE CONTRACTOR SHALL PROVIDE ONE-INCH STEEL PLATES TO PROVIDE FOR TRAFFIC MOVEMENT OVER NARROW, OPEN EXCAVATIONS. EXCAVATIONS MADE FOR THE INSTALLATION OF THE PIPES WILL BE BACKFILLED AT THE CLOSE OF EACH DAY. TEMPORARY PAVEMENT SHALL BE PLACED NO LATER THAN END OF WORK ON THE FOLLOWING DAY.
- 13. NO DROP-OFF GREATER THAN SIX INCHES SHALL BE LEFT OVERNIGHT WITHIN 30-FEET OF THE EDGE OF PAVEMENT. DROP-OFFS LESS THAN SIX INCHES SHALL BE PERMITTED IF PROPER DELINEATION AND SIGNING IS PROVIDED.
  - THE CONTRACTOR SHALL DELINEATE AREAS WHERE THERE IS A DROP-OFF NEAR THE EDGE OF THE TRAVELED WAY AND AREAS IN WHICH IT IS UNSAFE TO TRAVEL. THE PROVISIONS FOR DELINEATION SHALL BE IN ACCORDANCE WITH THE NYS STANDARD SPECIFICATIONS, SECTION 619-3.02J-CHANNELIZING DEVICES AND SECTION 619-3.02K-PAVEMENT EDGE DROP-OFF PROTECTION. THIS SECTION INDICATES MINIMUM DELINEATION REQUIREMENTS, ADDITIONAL DELINEATION MAY BE REQUIRED. REFLECTORIZED PLASTIC DRUMS SHALL BE USED AT HAZARDOUS LOCATIONS. DRUMS SHALL REMAIN IN PLACE UNTIL SATISFACTORY PROTECTION IS PROVIDED. DRUMS SHALL BE SPACED IN ACCORDANCE SECTION 619-3.02K, TABLE 619-3 OF THE NYS STANDARD SPECIFICATIONS-PAVEMENT EDGE DROP-OFF PROTECTION. THE CONTRACTOR MAY ELECT TO INSTALL TEMPORARY CONCRETE BARRIER TO ELIMINATE DROP-OFFS OR HAZARDS
  - ANY EXPOSED ROADSIDE HAZARDS OR EXCAVATIONS SHALL BE FENCED AND CHANNELIZED TO PREVENT PEDESTRIAN ACCESS, PARTICULAR ATTENTION TO OVERNIGHT HOURS.
- 14. W20-7a "FLAGGER" SIGNS SHALL BE USED WHENEVER FLAGGING OCCURS FOR MORE THAN A BRIEF PERIOD OF TIME. THE SIGNS SHALL BE PROMPTLY REMOVED, COVERED, OR FACED AWAY FROM TRAFFIC WHEN THE FLAGGING OPERATIONS CEASE.
- 15. THE FLAGGING STATIONS AND LAND CLOSURES SHOULD BE LOCATED TO ENSURE MAXIMUM VISIBILITY.
- 16. ALL FLAGGERS SHALL USE STOP/SLOW PADDLES (NO ROUND PADDLES) WHERE EVER PADDLES CAN BE USED. FLAGS SHOULD ONLY BE USED WHERE PADDLES MAY BECOME CONFUSING, ie. INTERSECTION CONTROL. STOP/SLOW PADDLES USED AS SIGNALING DEVICES SHALL COMPLY WITH SECTION 6E.03 HAND-SIGNALING DEVICES OF THE NATIONAL MUTCD AND THE NYS STANDARD SPECIFICATIONS SECTION 729-05. THE STOP/SLOW PADDLE SHALL BE A MINIMUM OF 24" (600mm) WIDE AND SHALL HAVE 8" (200mm) HIGH LETTERS.
- 17. SIDEWALKS SHALL BE REPLACED PRIOR TO MOVING TO THE OPPOSITE SITE TO WORK.
- 18. ALL SIGNS THAT ARE REMOVED DURING CONSTRUCTION OPERATION SHALL BE REINSTALLED AS SOON AS POSSIBLE (WITHIN THE SAME DAY). ANY DAMAGE TO SIGN PANEL OR POST SHALL BE REPAIRED AT NO COST TO THE AUTHORITY HAVING JURISDICTION. ANY SIGNS THAT NEED TO BE TEMPORARILY RELOCATED AS A RESULT OF THIS PROJECT MUST BE REPLACED IN THE CORRECT LOCATION, ELEVATION, AND AT THE CORRECT ANGLE TO TRAFFIC BY THE END OF THE DAY. THE BREAKAWAY SYSTEM ON ANY REPLACEMENT SIGNS SHALL MATCH THAT OF EXISTING SIGNS. IF AT ANY TIME A STOP SIGN MUST BE REMOVED, A FLAGGER SHALL BE USED TO CONTROL TRAFFIC AT THE INTERSECTION UNTIL THE STOP SIGN GETS REPLACED.
- 19. CARE SHALL BE TAKEN TO INSURE THAT NO DAMAGE OCCURS TO THE EXISTING PAVEMENT/SHOULDER/CURB AREAS AS A RESULT OF CONSTRUCTION EQUIPMENT MOVEMENT.





<u>LEGEND</u> △ CONES (CHANNELIZING DEVICE) WORK AREA SIGN (NATIONAL MUTCD)

∕—SHOULDER ′

ROAD WORK

NOTES:

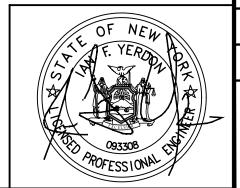
"A" = TRANSITION LENGTH, SEE TABLE "B" = BUFFER ZONE, SEE TABLE

4' TO 8' | 160' | 360' 10' TO 12' 240' 360'

THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES SHALL BE 40'. ALL SIGNS SHALL HAVE ORANGE BACKING WITH BLACK LETTERING AND TRIM. ANTICIPATED WORK DURATION, 1 HOUR TO 12 HOURS, DAYTIME NLY. THIS DETAIL IS FOR SHOULDER WORK ONLY, NOT FOR WORK IN TRAVEL LANE(S)

TRAFFIC CONTROL FOR SHOULDER CLOSURE ON ALL HIGHWAYS

NOT TO SCALE



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FOR RECORD

AUGUST 2021

FLAGGER SIGN (W20-7a) AND THE FLAGGER.

NOT TO SCALE

FLAGGING OPERATION

SHORT OR INTERMEDIATE TERM STATIONARY LANE CLOSURE

OR NEAR INTERSECTION

(SEE TABLE (SEE TABLE (SEE TABLE NY6H-3) TAPER(MAXIMUM)

WORK

CALL DIG SAFELY NEW YORK BEFORE DIGGING @ 1-800-962-7962

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WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT

12 E COUNTY ROUTE 1

REVISIONS

REV DESCRIPTION DAT FOR RECORD MARCH 2021

AUGUST 202

SHEET C-100

OF ABOVE GROUND FEATURES. THEIR LOCATION, SIZE AND ELEVATION MAY NOT BE ENTIRELY TRUE AND CORRECT. OTHER UTILITIES MAY EXIST. ALL LOCAL JTILITY COMPANIES SHOULD BE NOTIFIED BEFORE EXCAVATION.

ARCHITECTURE AND ENGINEERIN

architecture · engineering

WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT

COUNTY ROUTE 12 - PLAN & PROFILE

REVISIONS

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REV DESCRIPTION DAT

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OJ. #: 170003 TE: AUGUST 2021

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WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT

COUNTY ROUTE 12 - PLAN & PROFILE

REVISIONS
DESCRIPTION DATE

REV DESCRIPTION DATE
FOR RECORD
MARCH 2021

FOR RECORD

MARCH 2021

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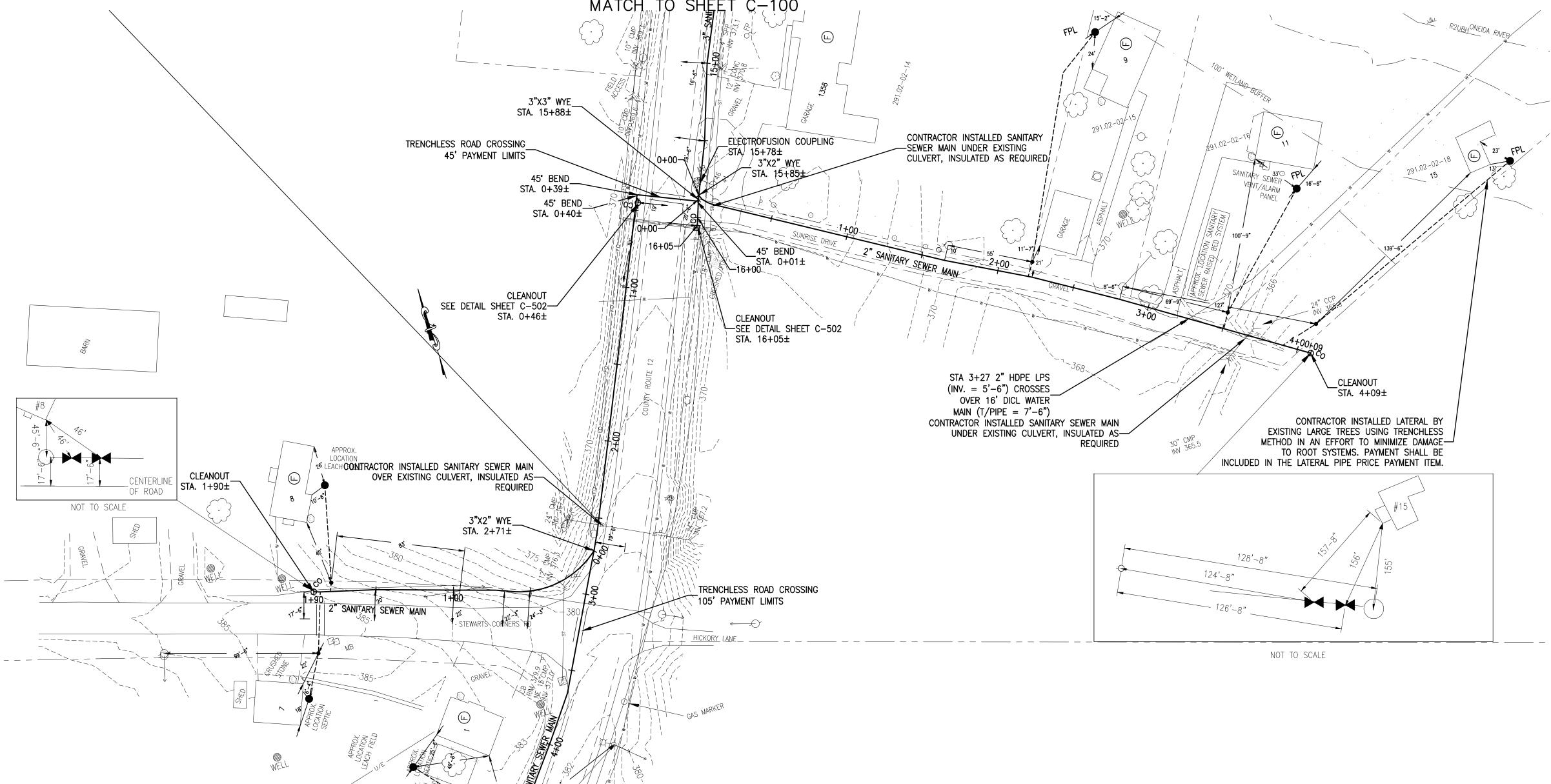
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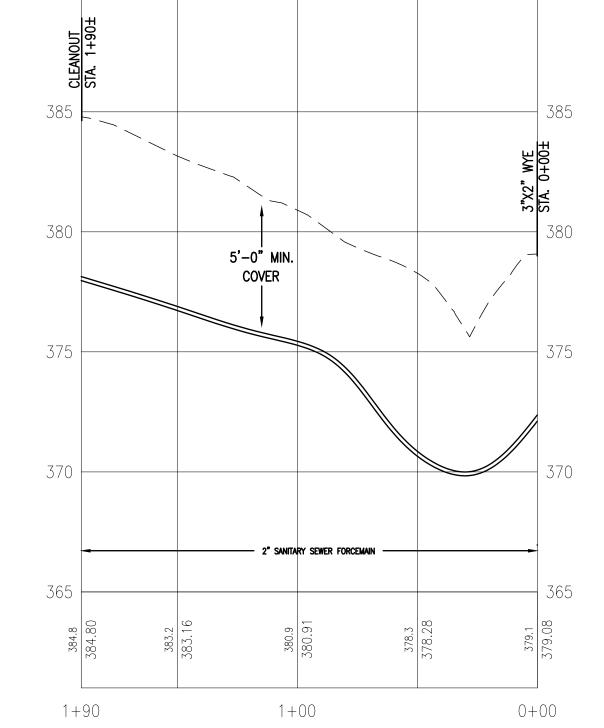
SHEET C-103



MATCH TO SHEET C-102 STEWARTS CORNERS ROAD & SUNRISE DRIVE PLAN

0+00

1+00



STEWARTS CORNERS ROAD PROFILE SCALE: 1"= 40' HORIZ. SCALE: 1"= 4' VERT.

EXISTING 8"
DIA. GAS

EXISTING 18"
DIA. STORM 5'-0" MIN. COVER EXISTING 30" DIA. STORM EXISTING 16" DIA. WATER 2" SANITARY SEWER FORCEMAIN

SUNRISE DRIVE PROFILE

SCALE: 1"= 40' HORIZ.
SCALE: 1"= 4' VERT.

3+00

2+00

ACCURATE INSTRUMENT SURVEY. HE LOCATION, SIZES AND ELEVATIONS OF THE EXISTING UTILITIES SHOWN N THIS DRAWING WERE OBTAINED FROM AVAILABLE DRAWINGS AND EVIDENCE F ABOVE GROUND FEATURES. THEIR LOCATION, SIZE AND ELEVATION MAY NOT E ENTIRELY TRUE AND CORRECT. OTHER UTILITIES MAY EXIST. ALL LOCAL TILITY COMPANIES SHOULD BE NOTIFIED BEFORE EXCAVATION.

THE PROPERTY LINES SHOWN ON THIS DRAWING WERE OBTAINED FROM

TRUE AND CORRECT AND ARE NOT INTENDED FOR USE IN THE CONVEYANCE

OF LAND. THE ACTUAL LOCATION OF THESE PROPERTY LINES IS SUBJECT TO SUCH VARIATIONS AND CORRECTIONS AS MIGHT RESULT FROM AN

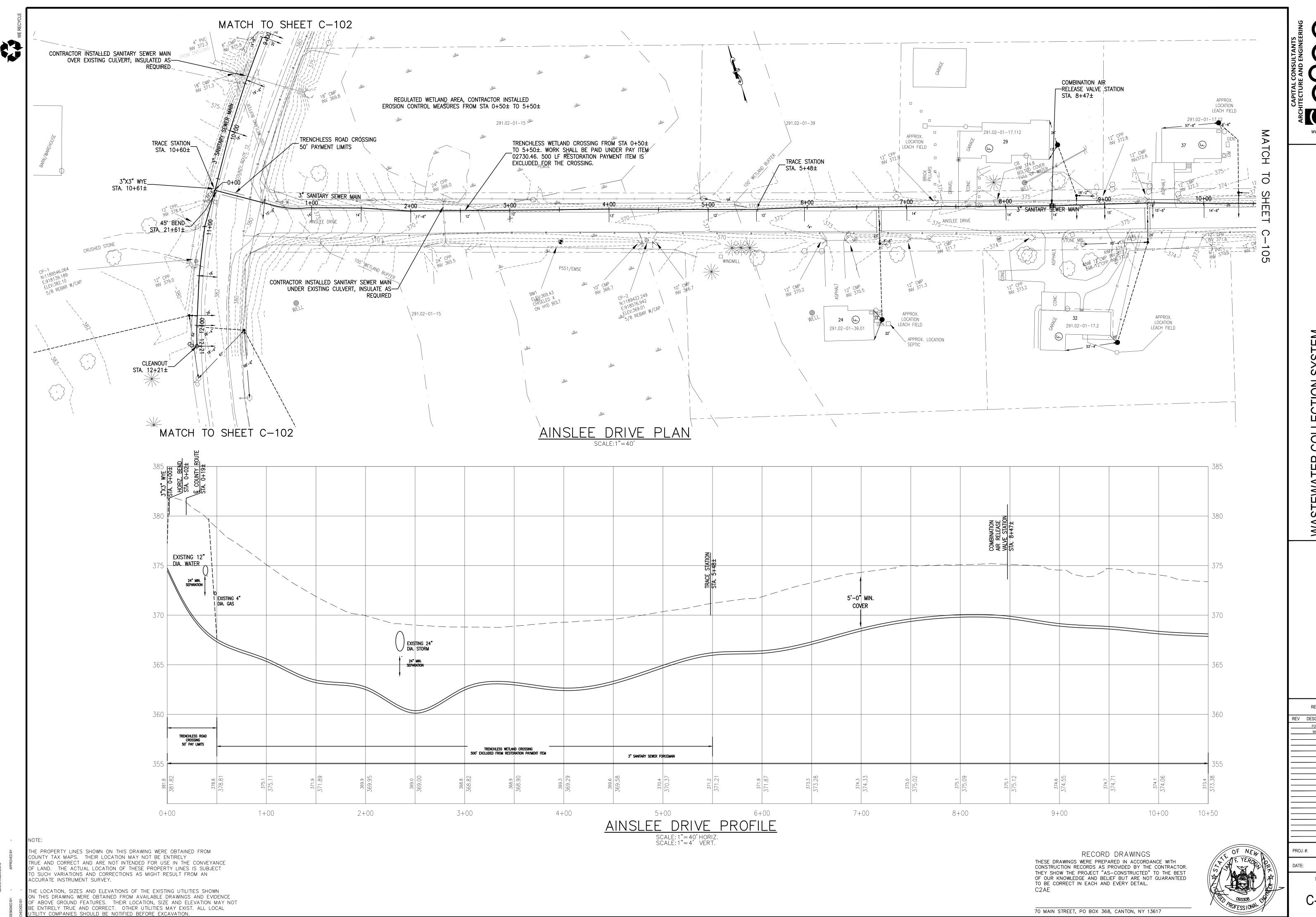
COUNTY TAX MAPS. THEIR LOCATION MAY NOT BE ENTIRELY

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WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT

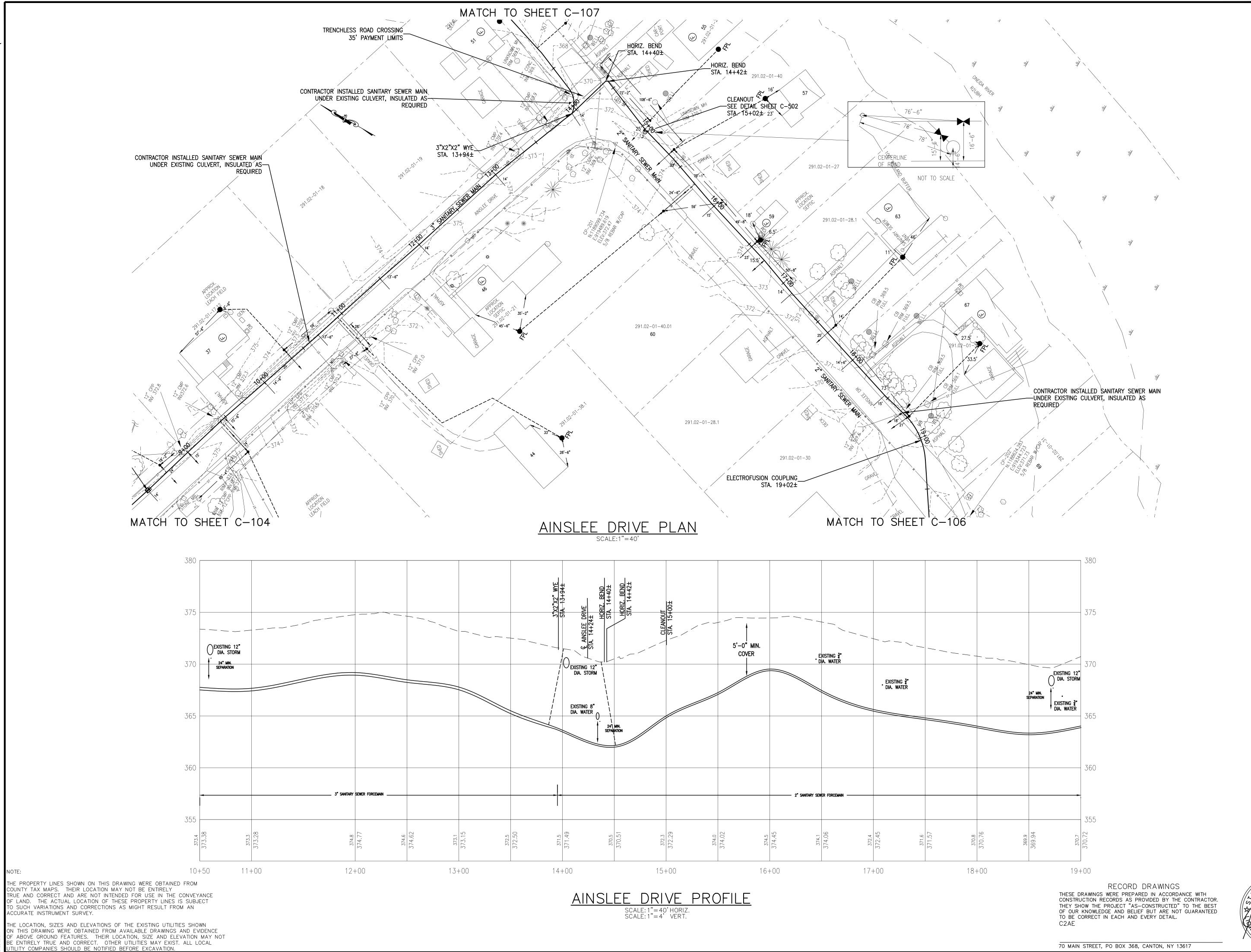
PROFILE DRIVE

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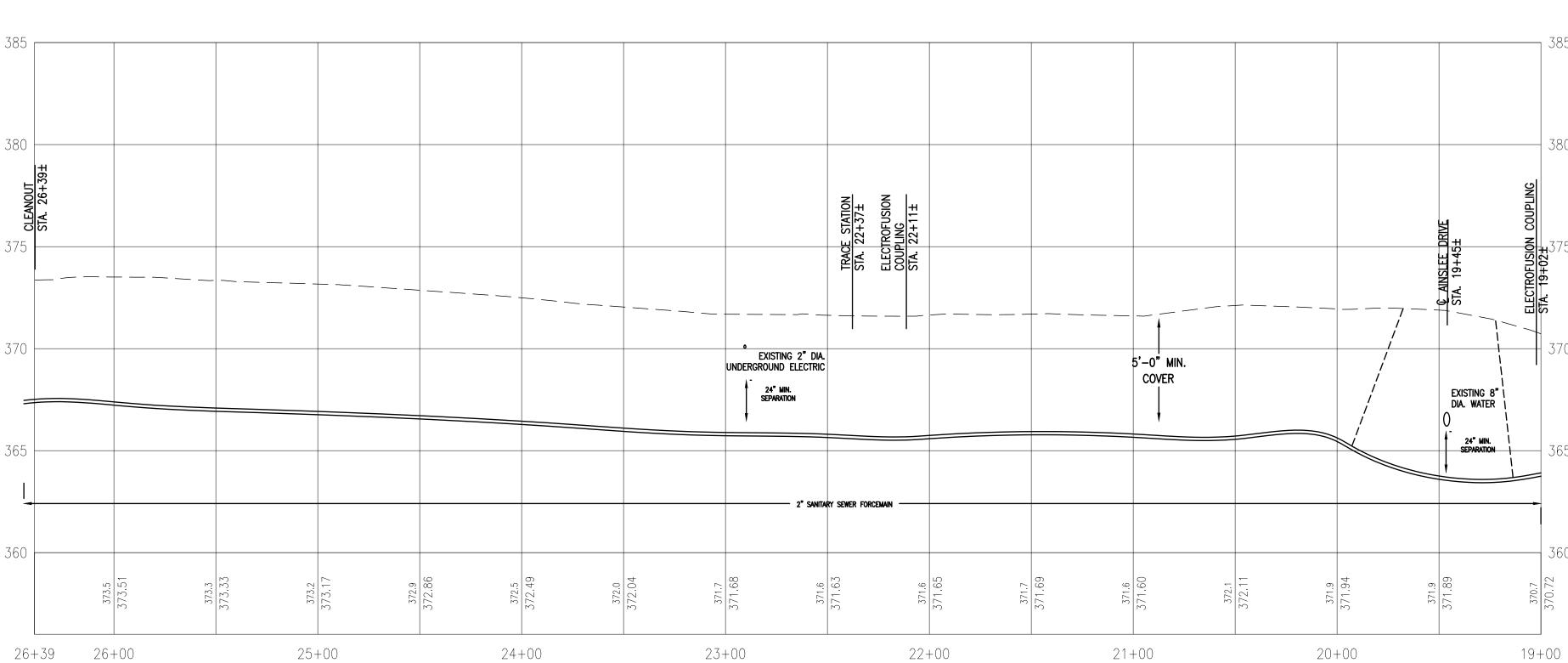
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REVISIONS

REV DESCRIPTION DAT FOR RECORD MARCH 2021

AUGUST 202 SHEET

C-105



AINSLEE DRIVE PROFILE

SCALE: 1"= 40' HORIZ.
SCALE: 1"= 4' VERT.

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SHEET

70 MAIN STREET, PO BOX 368, CANTON, NY 13617

NOTE:

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WASTEWATER COLLECTION SYSTEM AINSLEE DRIVE SEWER DISTRICT

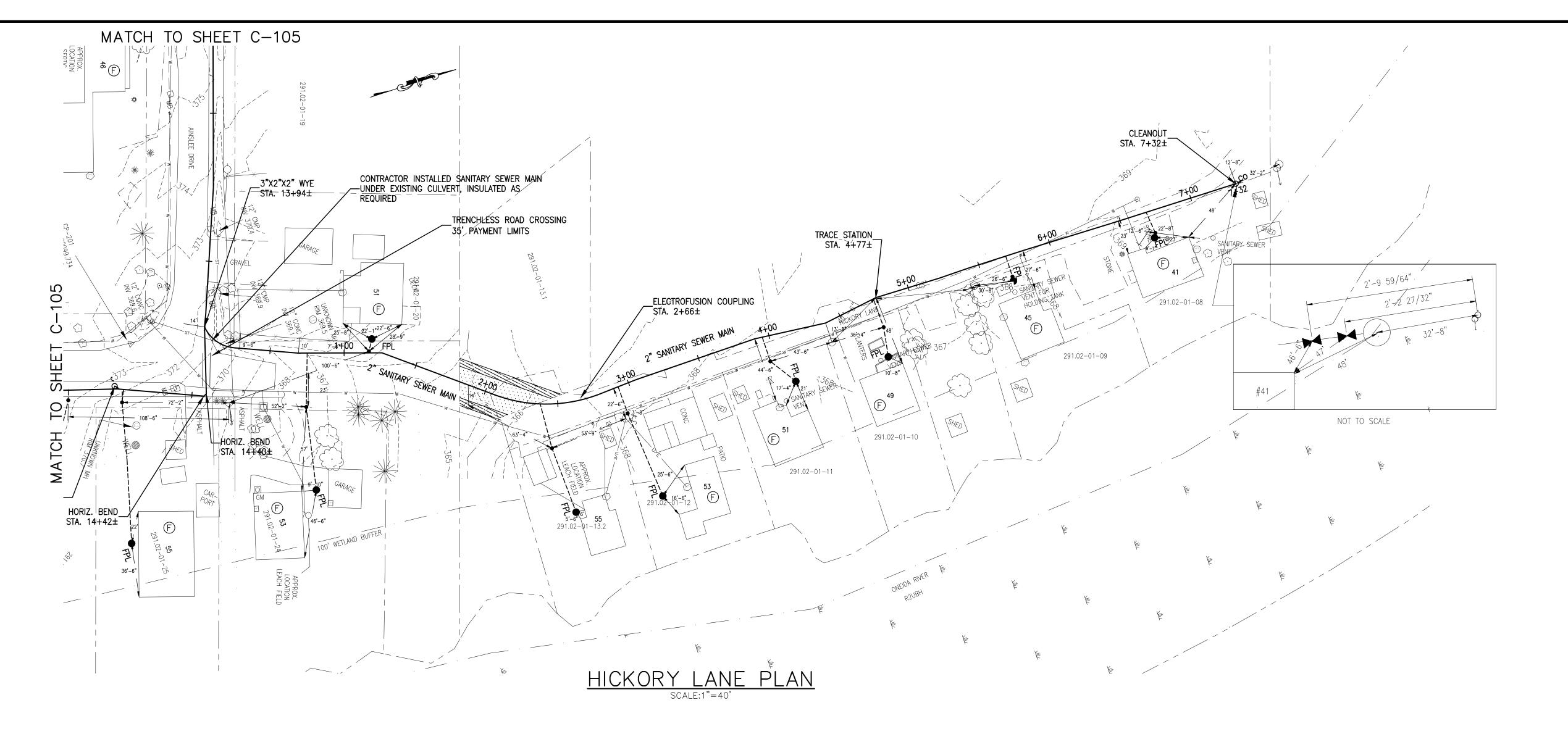
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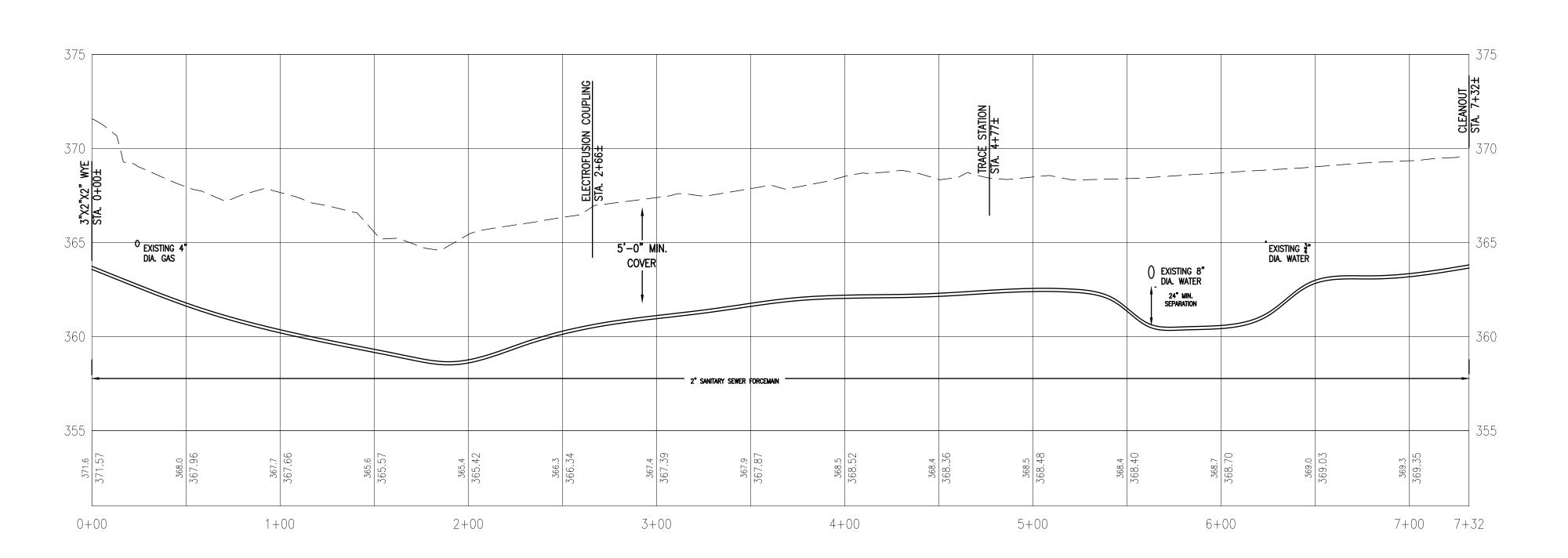
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HICKORY LANE PROFILE

SCALE: 1"= 40' HORIZ.
SCALE: 1"= 4' VERT.

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70 MAIN STREET, PO BOX 368, CANTON, NY 13617

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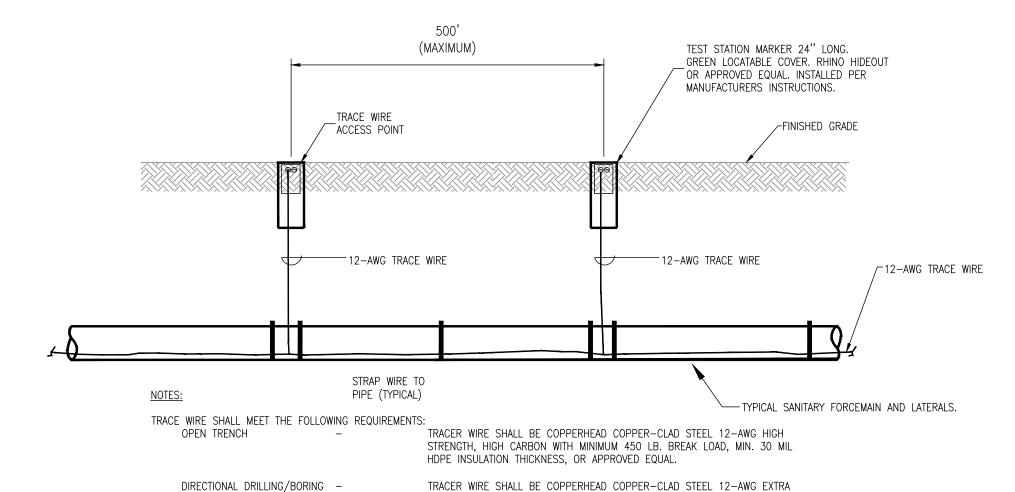
SHEET C-500

NOTE: USE INSULATION WHENEVER SANITARY SEWER MAIN OR SERVICE PIPING WILL PASS WITHIN TWO FEET OF A CULVERT PIPE OR IF DIRECTED BY ENGINEER STORM CULVERT SIZE AND TYPE VARY INSULATE SANITARY SEWER MAIN OR SERVICE PER TYPICAL SANITARY SEWER — 24" MIN. SEPARATION MAIN INSULATING DETAIL, SEE THIS SHEET BACKFILL WITH FLOWABLE FILL TO TOP OF STORM PIPE ALONG COUNTY ROADS. FLOWABLE FILL SHALL NOT BE LEFT EXPOSED IN BANK. PROPOSED SANITARY BACKFILL AREA SUPPORTING BELOW AND AROUND EXISTING CULVERT SEWER MAIN OR SERVICE PIPE WITH NO. 1 CRUSHED STONE. CRUSHED STONE SHALL BE COMPACTED TO 90% PROCTOR IN 6" MIN LIFTS

 ANY AND ALL SILT FENCE OR EROSION CONTROL MEASURES AT CULVERT CROSSING AREAS SHALL BE INSTALLED IMMEDIATELY (OR WITHIN THE SAME DAY) OF THE PROPOSED UTILITY BEING INSTALLED IN THE CULVERT AREA. SEE DETAIL "EROSION CONTROL AT CULVERT ENDS" ON DETAIL SHEET C-504 FOR DETAILS.

TYPICAL CULVERT CROSSING

VARIES



TRACER WIRE SHALL BE COPPERHEAD COPPER-CLAD STEEL 12-AWG EXTRA HIGH STRENGTH WITH MINIMUM 1,150 LB. BREAK LOAD, MIN. 45 MIL HDPE INSULATION THICKNESS, OR APPROVED EQUAL. ALL TRACE WIRE SHALL BE INTERCONNECTED AT INTERSECTIONS WITH A SINGLE, THREE-WAY LOCKING CONNECTOR, COPPERHEAD

CONTRACTOR SHALL STRAP TRACE WIRE TO THE BOTTOM HALF OF THE PIPE AND SECURED (TAPED/TIED) AT 5-FOOT INTERVALS. TRACE WIRE SHALL TERMINATE AT AN APPROVED TRACER WIRE ACCESS POINT COLOR CODED GREEN AND LOCATED DIRECTLY ABOVE THE FORCEMAIN EVERY 500' OR LESS. APPROVED TRACER WIRE ACCESS POINT SHALL ALSO BE USED AT LATERAL AND FOREMAIN

SNAKEBITE OR APPROVED EQUAL.

CONTRACTOR SHALL TEST WIRE BETWEEN ACCESS POINTS USING TYPICAL LOW FREQUENCY LINE TRACING EQUIPMENT, WITNESSED BY THE CONTRACTOR, ENGINEER, AND FACILITY OWNER AS APPLICABLE. CONTINUITY TESTING IN LIEU OF ACTUAL LINE TRACING SHALL NOT

CONTRACTOR SHALL COORDINATE WITH PIPING MANUFACTURER THE EXACT REQUIREMENTS FOR TRACE WIRE INSTALLATION. FACTORS SUCH AS PIPE EXPANSION INFLUENCE THE PROPER METHOD FOR INSTALLING TRACE WIRE. REFER TO SPECIFICATION "02730 - SANITARY SEWER SYSTEM" FOR ADDITIONAL REQUIREMENTS.

TYPICAL SANITARY SEWER COLLECTION PIPING TRACE WIRE

- RESTORE SURFACE TO MATCH ORIGINAL NOTE: PAVED DRIVEWAYS SHALL BE RESTORED TO A MINIMUM OF 2" BINDER AND 2" TOP OR MATCH EXISTING, WHICHEVER IS GREATER. 12" MIN. CRUSHER 12" MIN. OF 1" MINUS CRUSHER RUN STONE MECHANICALLY COMPACTED IN 6" LIFTS TO 95% PROCTOR COMPACTION SANITARY SEWER LINE (SEE TYPICAL SANITARY SEWER MAIN & SERVICE BEDDING DETAIL)

> TYPICAL DRIVEWAY CROSSING NOT TO SCALE

GRINDER PUMP STATION CONTROL PANEL (AND VENT ASSEMBLY\*\*), MOUNTED ON POST OR\_ — 6"x6"x10' PRESSURE TREATED EXTERIOR WALL OF RESIDENCE. CONTRACTOR TO COORDINATE LOCATION WITH PROPERTY OWNER PRIOR TO INSTALLATION. TYPICAL RESIDENTIAL DWELLING UNIT. DISTANCE FROM PUMP STATION WILL VARY. PERFORATED SS SCREEN\*\* 48" MINIMUM ∠ 2"PVC LATERAL VENT\*\* - STRAIN RELIEF CORD CONNECTOR\* 1" SCHEDULE 80 PVC CONDUIT SEE TYPICAL SANTARY SEWER LATERAL CONNECTION DETAIL <u>FOR CONTINUATION\*\*</u> ELECTRICAL QUICK DISCONNECT NEMA 6P PROVIDED WITH PUMP STATION TYPE TC DIRECT BURIAL, 12AWG, SIX CONDUCTOR-CONTROL CABLE 1" SCHEDULE 80 PVC CONDUIT. .-

THIS TYPICAL GRINDER PUMP STATION DETAIL IS A GENERAL OVERVIEW FOR THE REQUIREMENTS OF THE PROPOSED PUMP STATIONS. DUE TO THE DIFFERENCES AT EACH SITE THE CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH EACH PUMP STATION LOCATION. CONTRACTOR SHALL ALSO REFER TO SEWER LATERAL DRAWINGS. ALL CONDUIT PENETRATIONS SHALL BE THROUGH THE BOTTOM OF THE PUMP CONTROL PANEL, NO PENETRATIONS SHALL BE MADE THROUGH THE TOP OR SIDES, ALL CONDUIT PENETRATIONS SHALL BE WATER TIGHT AND SEALED TO AVOID MOISTURE AND MOISTURE VAPOR FROM ENTERING PANEL.

ALL CONDUIT SHALL BE MINIMUM 1" SCHEDULE 80 PVC AND ALL WIRING SHALL BE RATED FOR THE ENVIRONMENT IN WHICH IT IS INSTALLED. BURIED CONDUIT SHALL BE INSTALLED A MINIMUM 24" BELOW GRADE EXCEPT WHERE A DEEPER DEPTH IS REQUIRED BY NEC ARTICLE 300, TABLE 300-5. ALL WIRES SHALL BE MINIMUM #12 AWG. BRANCH CIRCUIT SIZES SHALL BE ADJUSTED FOR VOLTAGE DROP PER NEC REQUIREMENTS.

CONDUIT RUN AND BRANCH CIRCUIT WIRING LENGTHS SHALL VARY FOR EACH INSTALLATION. REFER TO DRAWING PLANS AND COORDINATE INSTALLATION WITH PROPERTY

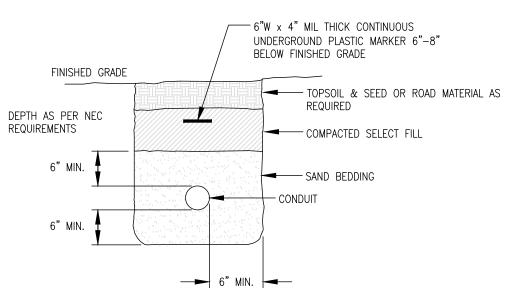
ALL BRANCH CIRCUITS SHALL BE TERMINATED AT RESIDENCE'S ELECTRICAL PANEL WITH A 30 AMP, 1-POLE CIRCUIT BREAKER.

REFER TO TRENCH DETAIL, FOR ADDITIONAL BURIED CONDUIT REQUIREMENTS.

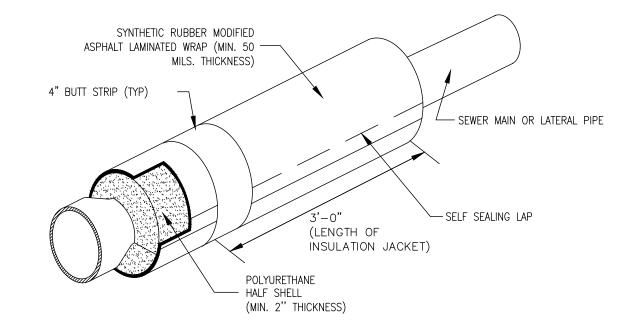
\*SEE TYPICAL SANITARY SEWER LATERAL CONNECTION FOR ILLUSTRATION OF FPL CONDUIT CONNECTION

\*\*VENT ASSEMBLY ONLY REQUIRED ON FPL GRINDER INSTALLATIONS

<u>typical electrical power detail</u>

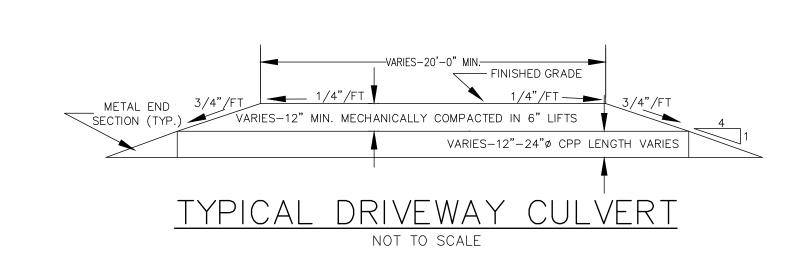


<u>typical electrical trench detail</u>



1. A JACKET ACCELERANT IS REQUIRED FOR INSTALLATION TEMPERATURES UNDER 50°F IN ORDER FOR ADHESIVES TO SEAL. 2. THE 3' INSULATION SECTION SHALL BE INSTALLED SUCH THAT THE JOINTS BETWEEN EACH 3' SECTION ARE NOT GREATER THAN  $\frac{1}{4}$ " OF EACH OTHER.

TYPICAL SEWER MAIN & LATERAL



RECORD DRAWINGS

THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH

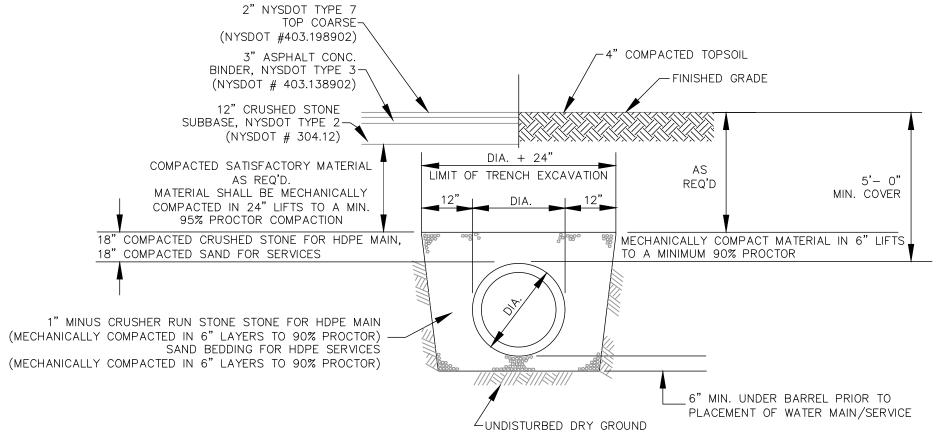
TO BE CORRECT IN EACH AND EVERY DETAIL.

70 MAIN STREET, PO BOX 368, CANTON, NY 13617

CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR.

THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST

OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED



NOTE: ALL SANITARY SEWER MAIN AND SERVICE PIPING SHALL BE INSTALLED IN A DRY TRENCH.

TYPICAL SANITARY SEWER MAIN & SERVICE LINE DETAIL

N.T.S.

## TYPICAL P.E. WALL ANCHOR

M.J. PIPE

--- CONCRETE ANCHOR

- PUSH ON JOINT

- SEE PLAN FOR SIZE, LOCATION AND

TOP OF GRATE TO BE 1" BELOW

-FINISHED GRADE

(GRASSED AREA)

-CONCRETE CURB

- BRICK COURSES BONDED W/

1/2 INCH MORTER MIN. 2

COURSES, 4 MAX.

" DIA. WEEP, COVERED WITH PERF. CAP, 1 EACH SIDE AT SUBGRADE

-COMPACTED CRUSHED STONE

EDGE OF PAVEMENT

& GRATE

ELEVATION.

°¹── 1'−6" TYP. PAY LIMIT COMPACTED SELECT FILL

BEDDING

INVERTS (TYP.)

P.E. PIPE

1. TEMPORARILY WRAP GRATE WITH FILTER FABRIC DURING CONSTRUCTION.

ANCHOR 2" X 3/16" X 4" WELDED TO

FRAME (4 REQUIRED)

PORTLAND CEMENT (TYP)

BASIN (ASTM 478)

GROUT WATERTIGHT

FOR SIZE SIZE AND

STORM PIPE, (SEE PLAN

SEE BEDDING DETAILS FOR PIPE-

BEDDING REQUIREMENTS

PRECAST CONCRETE CATCH

PAVEMENT MARKINGS.

2. GRATES SET IN PAVED AREAS SHALL BE PLACED PARALLEL OR PERPENDICULAR TO CURBS OR

CATCH BASIN-

ASPHALT BINDER

UNDISTURBED GROUND-

LINKSEAL BETWEEN PIPE\_ AND PENETRATION

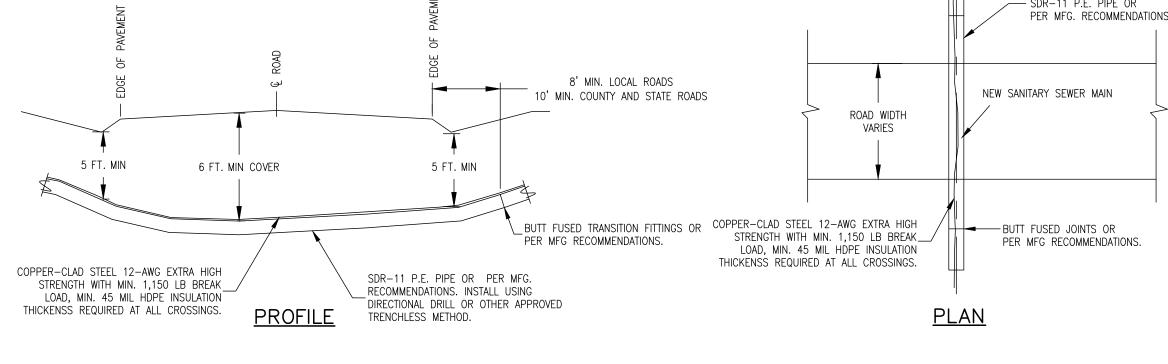
TRANSITION FROM

6" MIN.

6" MIN.

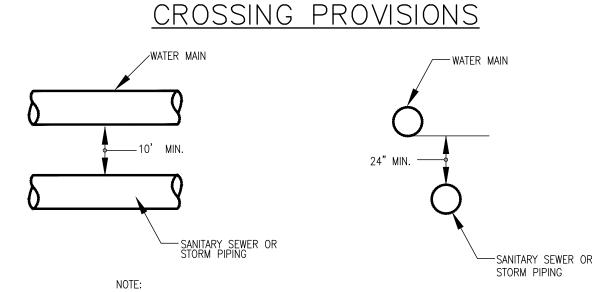
**SECTION** 

FRAME & GRATE ---



• SANITARY SEWER DISTRIBUTION TRACE WIRE TEST STATION MARKERS ARE REQUIRED FOR EACH END OF A TRENCHLESS ROAD CROSSING.

## TYPICAL TRENCHLESS ROAD CROSSING



ONE FULL LENGTH OF WATERMAIN SHALL BE CENTERED UNDER OR OVER THE SEWER OR STORM SUCH THAT BOTH ENDS ARE LOCATED AS FAR FROM THE SEWER OR STORM AS POSSIBLE.

SPECIAL STRUCTURAL SUPPORT FOR THE WATER, SEWER OR STORM PIPES MAY BE REQUIRED BY

STORM PIPING

THE ENGINEER.

SEWERS SHALL BE LAID AT LEAST 10' (3 m) HORIZONTALLY FROM ANY EXISTING OR PROPOSED WATER MAIN. THE DISTANCE SHALL BE MEASURED EDGE TO EDGE. IF 10' SEPARATION IS NOT POSSIBLE, THE SEWER WILL BE INSTALLED SO THAT THE WATER MAIN IS IN A SEPARATE TRENCH OR ON AN UNDISTURBED EARTH SHELF LOCATED ON ONE SIDE OF THE SEWER. THE BOTTOM OF THE WATER MAIN WILL BE AT LEAST 24" ABOVE THE TOP OF THE SEWER MAIN.

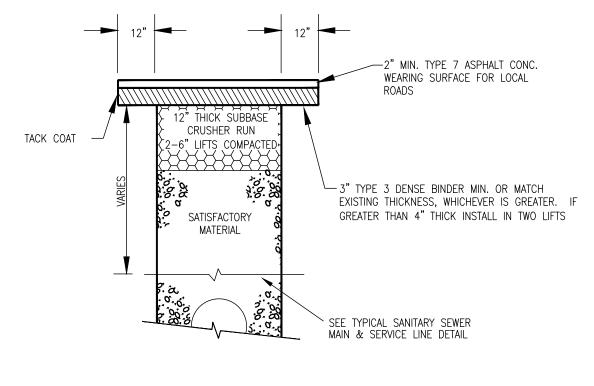
WATER MAIN

IF BOTH HORIZONTAL AND VERTICAL SEPARATION CAN NOT BE ACHIEVED AS DESCRIBED ABOVE, BOTH THE WATER MAIN AND THE SEWER MAIN WILL BE CONSTRUCTED OF SLIP-ON OR MECHANICAL JOINT PIPE COMPLYING WITH PUBLIC WATER SUPPLY DESIGN STANDARDS AND BE PRESSURE TESTED TO 150 PSI TO ASSURE WATER TIGHTNESS BEFORE BACKFILLING.

#### HORIZONTAL AND VERTICAL SEPARATION PROVISIONS WATER/SEWER/STORM CROSSING AND SEPARATION DETAILS

RECORD DRAWINGS THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED TO BE CORRECT IN EACH AND EVERY DETAIL.

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COUNTY & LOCAL PAVED

TYPICAL ROAD PAVEMENT RESTORATION

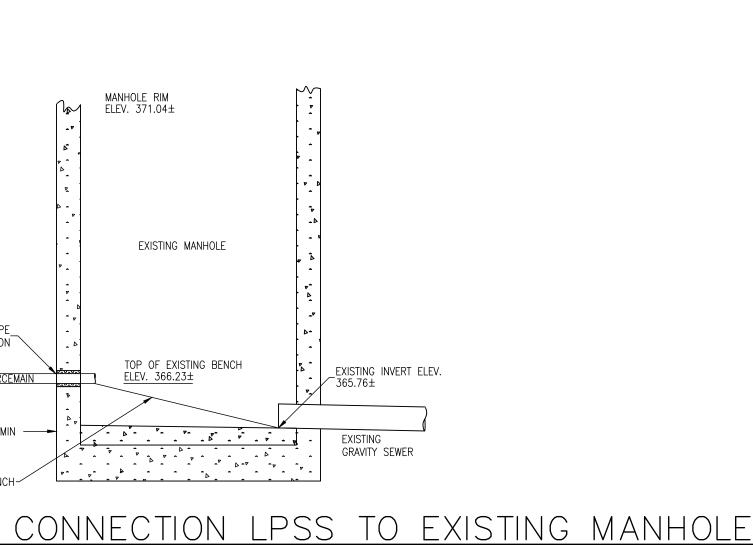
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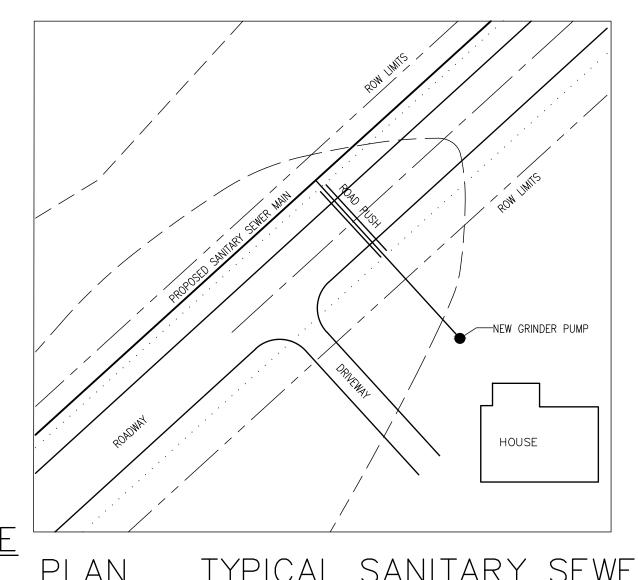
SCALE: 1"=50' HORIZ.

# STORM INLET DETAIL

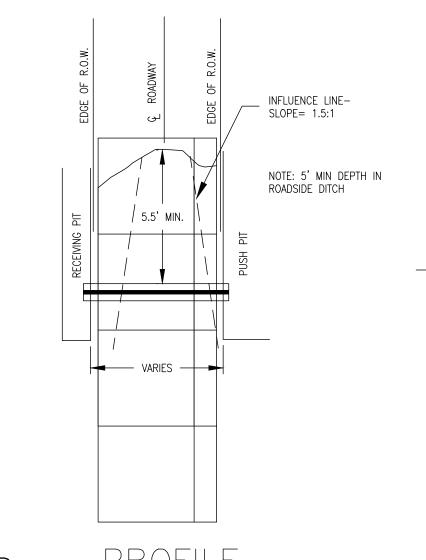
EXISTING MANHOLE

TOP OF EXISTING BENCH

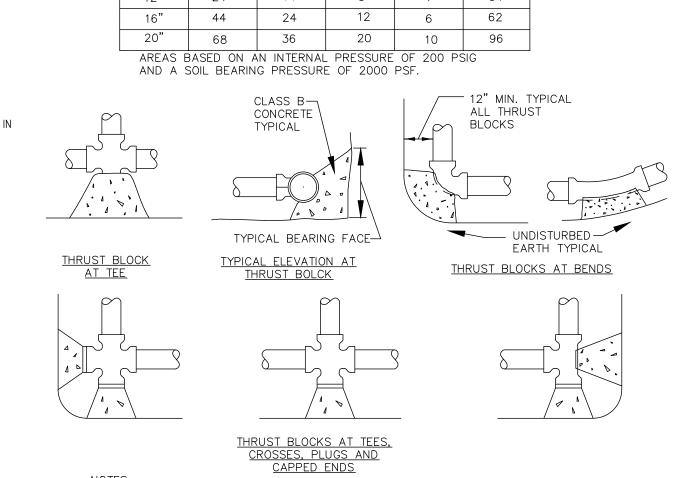








SCALE: 1"=50' HORIZ. SCALE: 1"=5' VERT.

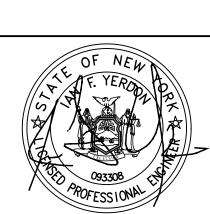


AREA OF BEARING FACE OF CONCRETE THRUST BLOCK IN SQ. FT. BLOCKS TO BE POURED AGAINST UNDISTURBED EARTH

PIPE SIZE 90° BEND 45° BEND 22.5° BEND 11.25° BEND 1

1. THRUST BLOCKS SHALL BE PLACED AT ALL BENDS, TEES, CAPPED ENDS, ETC IN THE SANITARY SEWER MAIN.

N.T.S.



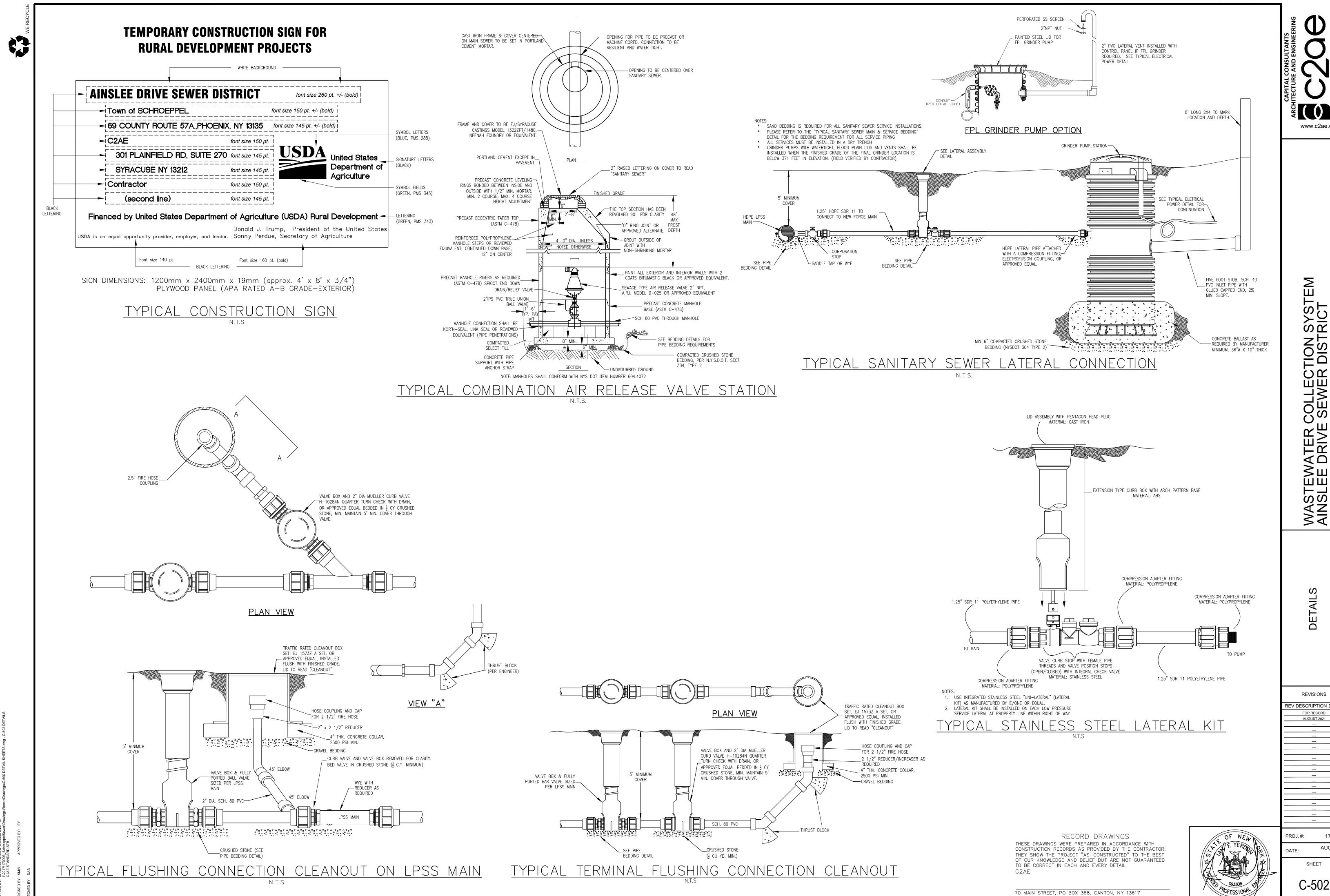
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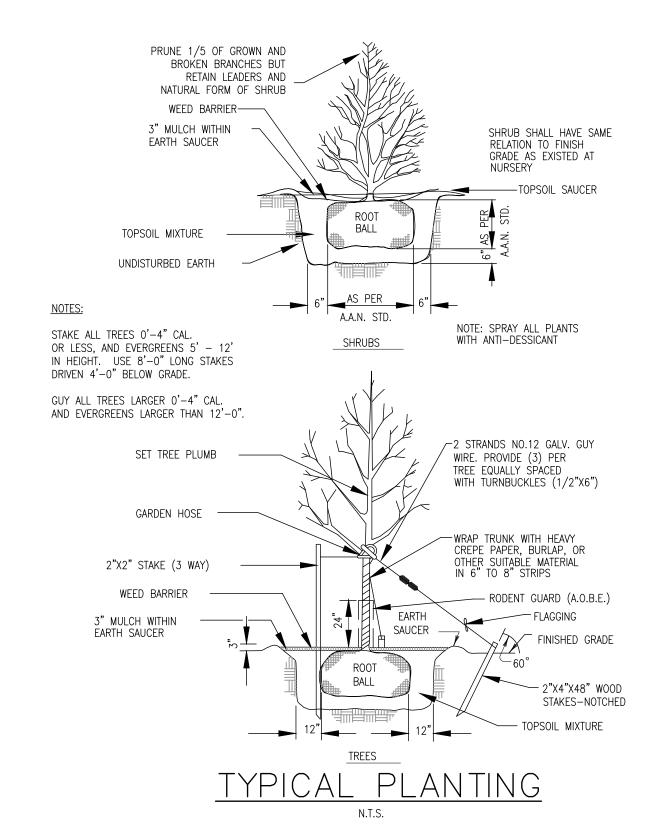
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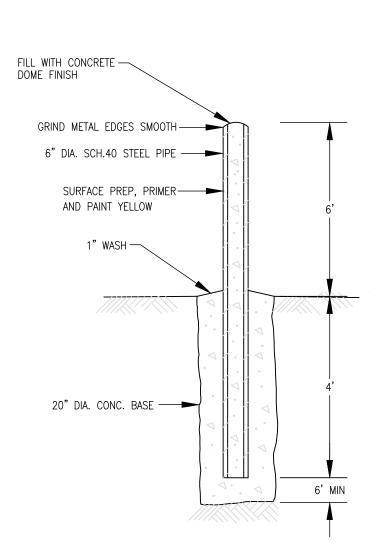
SHEET

EDGE OF STREAM/WETLAND EDGE OF STREAM/WETLAND · VARIES · COPPER-CLAD STEEL 12-AWG EXTRA HIGH STRENGTH WITH MIN. TOP OF BANK - 1,150 LB BREAK LOAD, MIN. 45 MIL - STREAM BOTTOM HDPE INSULATION THICKNESS REQUIRED AT ALL CROSSINGS. 6 FT MIN COVER SDR-11 P.E. PIPE OR PER MFG. RECOMMENDATIONS. INSTALL USING DIRECTIONAL DRILL OR OTHER APPROVED TRENCHLESS METHOD. **PROFILE** 

 A MINIMUM OF 5 FULL PIPE LENGTHS (FULL LENGTH BEING 18' OR 20') SHALL BE RESTRAINED WITH LOCKING RESTRAINT
GASKETS OR BELL CLAMP RESTRAINTS WHEN CONNECTING TO HDPE WATER MAIN. THIS IS REQUIRED EVEN IF CONCRETE PE
WALL ANCHORS ARE ALSO INSTALLED. REFER TO SHEET G-003 GENERAL NOTE #10 FOR MORE DETAILS. ANY AND ALL SILT FENCE OR EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY TRENCHLESS INSTALLATION

TYPICAL TRENCHLESS STREAM/WETLAND CROSSING

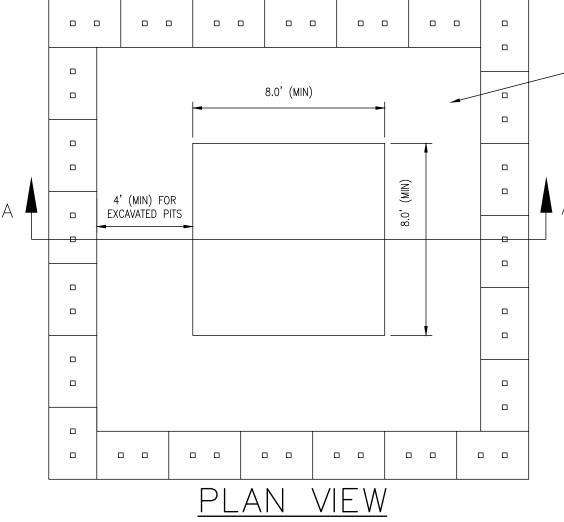


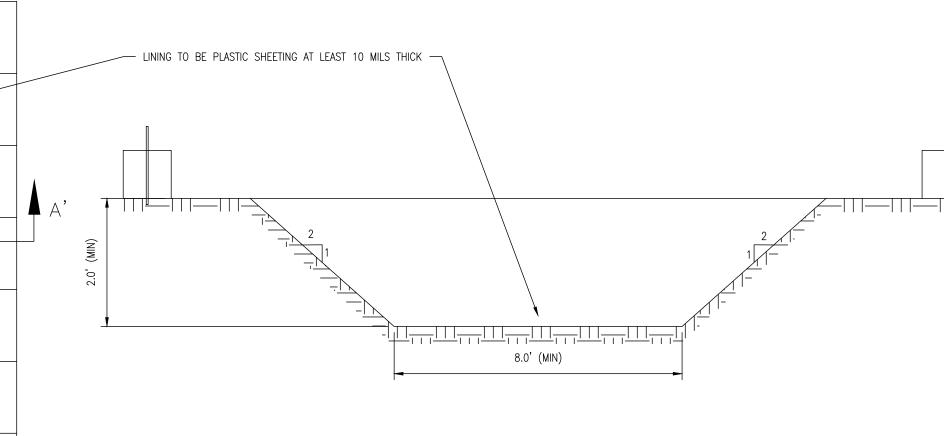


SECTION IN EARTH OR AGGREGATE

TYPICAL BOLLARD

EDGE OF PAVEMENT





SECTION A-A'

### TYPICAL CONCRETE WASHOUT PIT

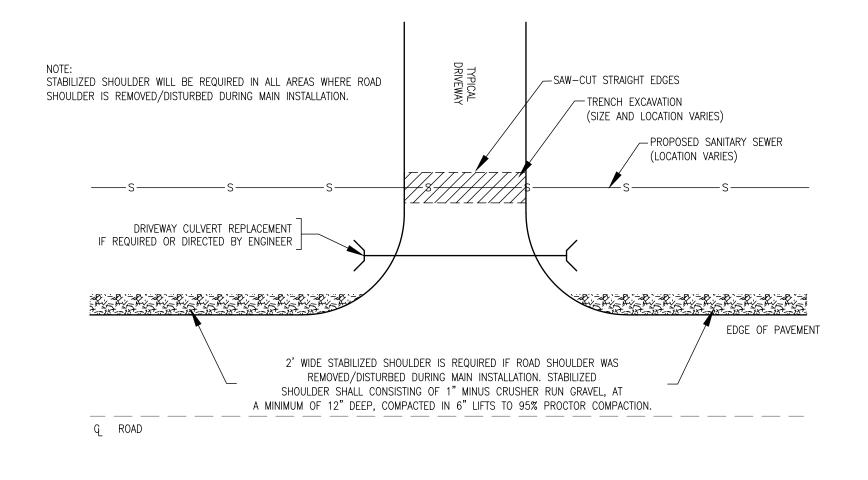
 CAPACITY TO BE SIZED TO THE MINIMUM DIMENSIONS ABOVE OR TO CONTAIN SOLIDS, WASH WATER, AND RAIN WATER TRIBUTARY TO THE PIT, WHICHEVER IS LARGER. WASH WATER TO BE ESTIMATED AS 7 GALLONS PER CHUTE AND 30 GALLONS

PER HOPPER OF THE CONCRETE TRUCK. ACCUMULATED HARDENED MATERIAL IS TO BE REMOVED BEFORE THE WASHOUT PIT REACHES 75% OF STORAGE CAPACITY AND

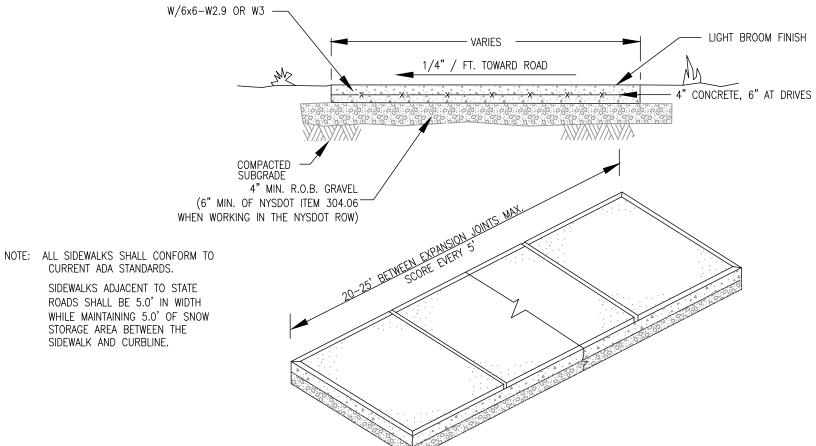
DISPOSED OF IN AN OFFSITE LANDFILL. WASH WATER IS TO BE BROUGHT OFF SITE IN PROPER CONTAINMENT VESSEL.

 DISPOSAL OF HARDENED MATERIAL IS NOT PERMITTED ON SITE REPLACE PLASTIC LINER AFTER EACH CLEANING OF THE WASHOUT PIT.

 CONSTRUCTION MUST BE IN ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE GUIDELINES FOR URBAN EROSION & SEDIMENT CONTROL, STANDARD SPECIFICATIONS FOR CONCRETE WASHOUT PIT.



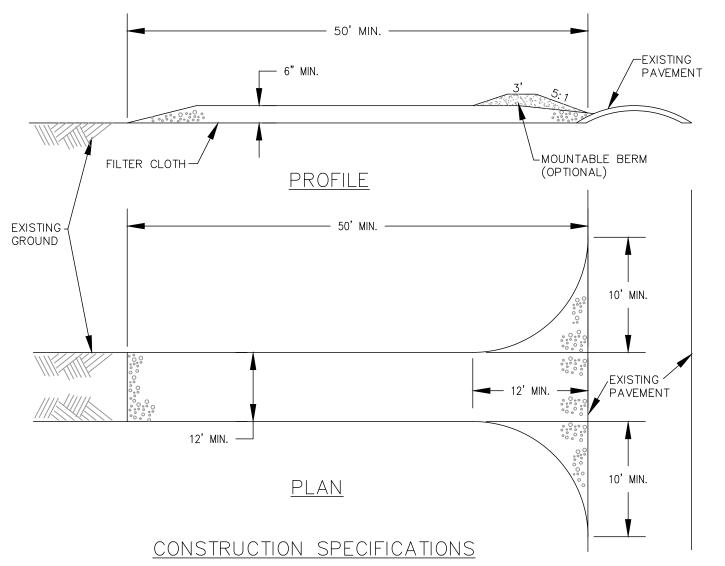
TYPICAL ROAD SHOULDER RESTORATION



RECORD DRAWINGS THESE DRAWINGS WERE PREPARED IN ACCORDANCE WITH CONSTRUCTION RECORDS AS PROVIDED BY THE CONTRACTOR. THEY SHOW THE PROJECT "AS-CONSTRUCTED" TO THE BEST OF OUR KNOWLEDGE AND BELIEF BUT ARE NOT GUARANTEED TO BE CORRECT IN EACH AND EVERY DETAIL.

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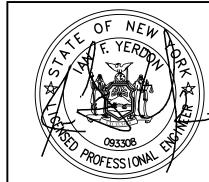
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- 1. STONE SIZE— USE 2" STONE, OR RECLAIMED OR RECYCLED CONCRETE EQUIVALENT.
- 2. LENGTH- NOT LESS THAN 50 FEET (EXCEPT ON A SINGLE RESIDENCE LOT WHERE A 30 FOOT MINIMUM LENGTH WOULD APPLY.)
- 3. THICKNESS- NOT LESS THAN SIX (6) INCHES.
- 4. WIDTH- TWELVE (12) FOOT MINIMUM, BUT NOT LESS THAN FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS. TWENTY-FOUR (24) FOOT IF SINGLE ENTRANCE TO SITE.
- 5. FILTER CLOTH- WILL BE PLACED OVER ENTIRE AREA PRIOR TO PLACING OF STONE.
- 6. SURFACE WATER- ALL SURFACE WATER FLOWING OR DIVERTED TOWARD CON- STRUCTION ENTRANCES SHALL BE PIPED ACROSS THE ENTRANCE. IF PIPING IS IMPRACTICAL, A MOUNTABLE BERM WITH 5:1 SLOPES WILL BE PERMITTED.
- 7. MAINTENANCE- THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. ALL SEDIMENT SPILLED, DROPPED, WASHED OF TRACTED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED
- 8. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
- 9. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.
- 10. CONSTRUCTION MUST BE IN A ACCORDANCE WITH THE LATEST VERSION OF THE NEW YORK STATE GUIDELINES FOR URBAN EROSION & SEDIMENT CONTROL, STANDARD SPECIFICATIONS FOR STABILIZED CONSTRUCTION ENTRANCE.

STABILIZED CONSTRUCTION

ENTRANCE DETAIL



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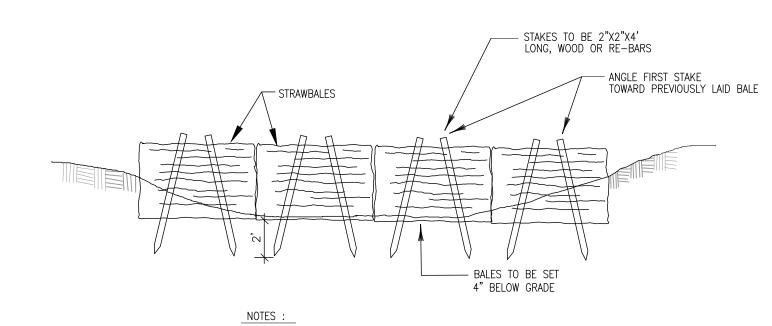
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AUGUST SHEET

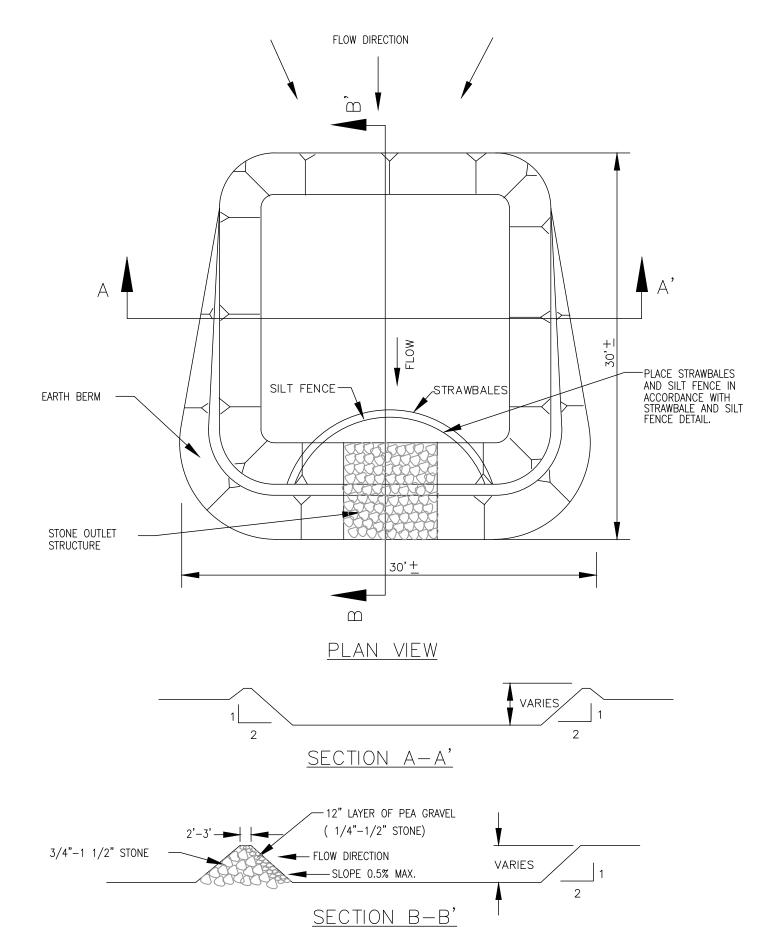
### TYPICAL STORM INLET PROTECTION

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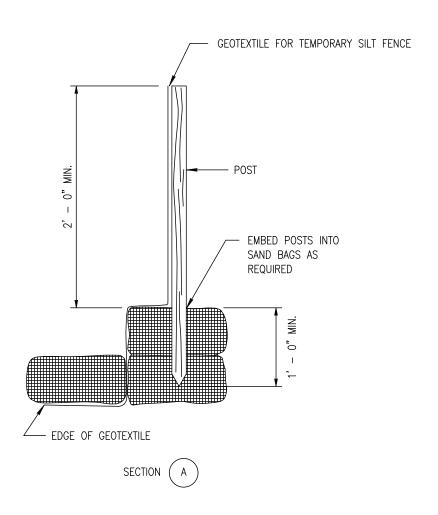


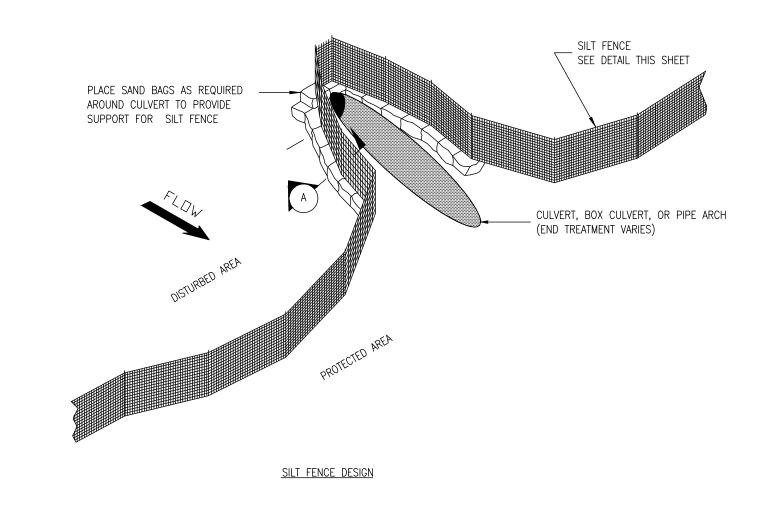
1. STRAWBALES TO BE REPLACED AS NECESSARY DUE TO DAMAGE OR FILLED WITH SILT. SILT TO BE REMOVED IN FRONT OF BALES REGULARLY TO PREVENT EXCESSIVE SOIL BEARING WEIGHT ON THE BALES. 2. STRAWBALES ARE NOT ALLOWED WITHIN THE NYSDOT RIGHT-OF-WAY

#### TYPICAL STAKED STRAWBALE N.T.S.



TYPICAL TEMPORARY SILT BASIN



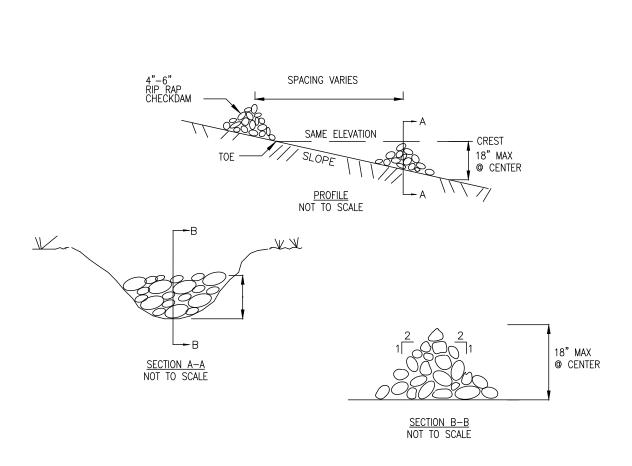


TYPICAL EROSION CONTROL AT CULVERT ENDS

#### STORMWATER POLLUTION PREVENTION NOTES

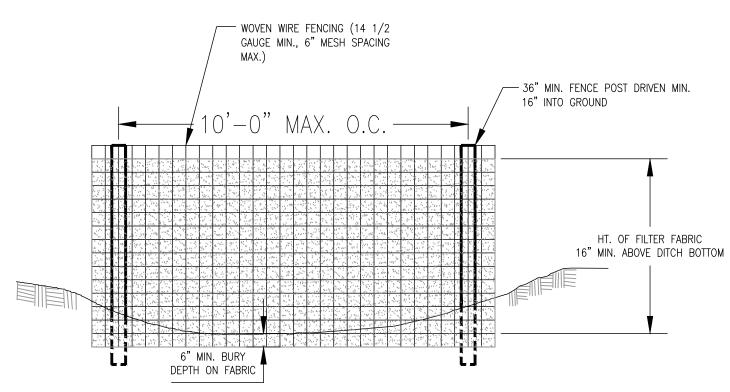
- 1. THE CONTRACTOR SHALL NOT HAVE MORE THAN FIVE (5) ACRES OF UNPROTECTED SOIL AT ONE TIME. THE CONTRACTOR SHALL PROTECT ANY DISTURBED AREAS FROM EROSION WITHIN 14 DAYS OF DISTURBANCE.
- EROSION CONTROL MEASURES INCLUDING SILT FENCE SHALL BE INSTALLED PRIOR TO THE START OF CONSTRUCTION . THE CONTRACTOR SHALL INCORPORATE EROSION CONTROL MEASURES ON DISTURBED AREAS WITH SLOPES OF 3:1 OR
- GREATER OR IN CONCENTRATED FLOW PATHS. FOR DISTURBED AREAS WITH SLOPES OF 3:1 OR LESS, TEMPORARY MULCHING SHALL BE APPLIED AT AN APPLICATION RATE OF 100 LBS. PER 1000 sq.ft. 5. THE CONTRACTOR SHALL PREPARE AND CONTINUOUSLY UPDATE A CONSTRUCTION PHASING PLAN FOR EROSION AND SEDIMENT
- 6. THE LOCATIONS FOR EQUIPMENT, WASTE, BORROW, AND OFF-SITE MATERIAL SHALL BE PROVIDED BY THE CONTRACTOR.
- 7. THE CONTRACTOR SHALL PROTECT SOIL AND STONE STOCKPILES FROM EROSION AT THE END OF EACH WORK DAY.

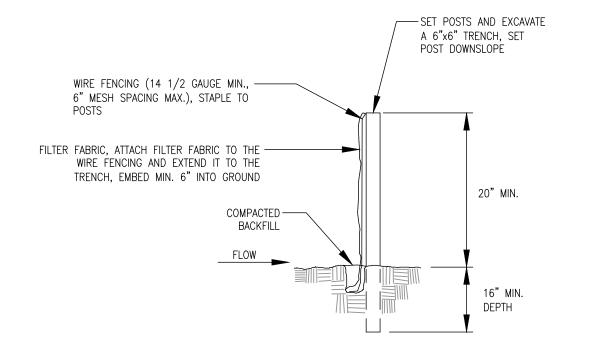
  8. THE CONTRACTOR SHALL CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE AT STAGING AREAS TO PREVENT THE TRACKING
- OF SEDIMENT ON TO PUBLIC STREETS. 9. THE CONTRACTOR SHALL PERFORM WEEKLY AND POST RAINFALL INSPECTIONS OF ALL EROSION AND SEDIMENT CONTROL
- 10. THE CONTRACTOR SHALL MAINTAIN EROSION AND SEDIMENT CONTROL PRACTICES UNTIL PERMANENT RESTORATION IS APPROVED BY AGENCY HAVING JURISDICTION.



TYPICAL CHECK DAM

 ANY AND ALL SILT FENCE AND EROSION CONTROL MEASURES SHALL BE INSTALLED PRIOR TO ANY AND ALL CONSTRUCTION ACTIVITIES, INCLUDING INSTALLATION OF SEWER MAINS AND LATERALS





1. CONSTRUCTION MUST BE IN ACCORDANCE WITH NEW YORK STATE GUIDELINES FOR URBAN EROSION & SEDIMENT CONTROL, STANDARD SPECIFICATIONS FOR SILT FENCE, LATEST VERSION

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